

TAMALPAIS AREA COMMUNITY PLAN

APPENDICES

September, 1992

**APPENDIX A
PROGRAM IMPLEMENTATION MATRIX**

APPENDICES

- A. PROGRAM AND IMPLEMENTATION MATRIX
- B. RESIDENTIAL FLOOR AREA RATIO FORMULA
- C. BOTHIN MARSH BUFFER ZONE
- D. SHORELINE AND MANZANITA AREAS DEVELOPMENT
- E. TRANSPORTATION
- F. ADMINISTRATIVE POLICIES
- G. STEERING COMMITTEE ROSTER
- H. PARCELS WITH SUBDIVISION POTENTIAL
- I. TAM JUNCTION DESIGN GUIDELINES
- J. ORDINANCES
- K. PARCELS ALONG SHORELINE HIGHWAY DESIRABLE FOR OPEN SPACE

IMPLEMENTATION SUMMARY

ISSUE	OBJECTIVE	PROGRAM
COMMUNITY WIDE ISSUES		
Preservation of Community Character and Image	LU.1 Preserve and enhance the natural and built characteristics of each neighborhood.	<p>LU1.1a. Study to identify visually prominent ridgelines. Prohibit construction within 100 feet of the ridge.</p> <p>LU1.1b Design dwellings so that rooftop is below ridgeline.</p> <p>LU1.2a Include standard for preservation of portion of a parcel in a natural state.</p> <p>LU1.2b Avoid removal of trees and rare plant communities.</p> <p>LU1.3a. Develop Hillside Design Guidelines.</p> <p>LU1.4a F.A.R. limitations for hillside lots and properties which do not meet minimum size standards.</p> <p>LU1.4b On hillside lots, the building shall not exceed 30 ft. above natural grade.</p> <p>LU1.4c Lowest floor of structure should not exceed 10 feet above natural areas.</p> <p>LU1.4d Design guidelines for residential development.</p>
Residential Densities and Rezoning	LU.2 Establish densities compatible with environmental constraints.	<p>LU2.1a Identify land with subdivision potential.</p> <p>LU2.1b Relate development density to capacity of needed services.</p> <p>LU2.1c Development should occur according to criteria in the Countywide Plan.</p> <p>LU2.1d New densities based on the sites topographic and geologic conditions.</p> <p>LU2.1e Design review shall be required as a condition of tentative map approval.</p> <p>LU2.2a The programs above (LU2.1a-e) also implement Policy LU2.2.</p> <p>LU2.3a. Re-zone parcels R-3:G-2, R-3:G-3, and R-3:G-4.</p>

<p>Development in Historic Subdivisions</p>	<p>LU.3 Regulate development so that density, intensity, location and form of residential development is responsive to site constraints.</p>	<p>LU3.1a Amend Subdivision Ordinance to modify and clarify the definition of a subdivision.</p> <p>LU3.2a Require design review for homes on substandard lots.</p> <p>LU3.2b Development shall be consistent with the community plan.</p> <p>LU4.1a Meet with property owners to encourage lot mergers.</p>
<p>Housing Variety and Affordability.</p>	<p>LU.5 Maintain a variety of housing accommodations.</p>	<p>LU5.1a Support tax relief which will reduce the spiraling cost of housing</p> <p>LU5.1b Support leased housing programs.</p> <p>LU5.1c Encourage non-profit housing organizations to purchase property.</p> <p>LU5.1d Encourage co-housing and shared housing.</p> <p>LU6.1a Implement second unit ordinance standards.</p> <p>LU6.1b Track all new second units and legalization of second units.</p>
<p>Historic Subdivision on Paper Streets</p>	<p>LU.7 Regulate development served by paper streets.</p>	<p>LU7.1a Definition of a paper street should not be changed.</p> <p>LU7.2a Enforce provisions of the paper street ordinance.</p>
<p>Preservation of Cultural and Archaeological Resources</p>	<p>LU.8 Preserve archeological and cultural resources.</p>	<p>Policy LU8.1 and LU8.2 encourage protection of resources.</p>
<p>Maintenance of a local serving commercial base</p>	<p>LU.9 Develop a mix of community-serving, small scale, commercial uses.</p>	<p>LU9.1a Establish a zoning district which promotes resident or local serving businesses.</p> <p>LU9.2a Home occupations shall be required to meet standards in the Marin County Code.</p>

<p>Preservation of Wetlands and Bay Waters</p>	<p>LU.10 Protect waters and marshlands of upper Richardson Bay and the adjacent shoreline areas.</p>	<p>LU10.1a Establish a marshside preserve adjacent to Bothin Marsh.</p> <p>LU10.1b Through a variety of mechanisms, ensure protection of the waters and marshlands of Richardson Bay.</p> <p>LU10.1c The County shall implement a park fee on new subdivisions.</p> <p>LU10.2a Provide habitat buffer zones to protect the habitat value of wetlands and bay waters.</p> <p>LU10.2b Limit intrusive access along salt marsh edges with buffers, fences or appropriate signage.</p> <p>LU10.3a When development is proposed in the Bothin Marsh Area, or Shoreline area, a special study should be done by a qualified biologist.</p>
<p>Protection of Stream Courses and Riparian Zones</p>	<p>LU.11 Protect the stream corridors and drainage network.</p>	<p>LU11.1a Implement existing Countywide Plan policies for stream setbacks to protect stream corridors and banks.</p> <p>LU11.2a Identify damaged reaches of streams and target for restoration or stabilization, in conjunction with permits for new construction or alteration.</p> <p>LU11.2b Retain unimproved water courses so that they are natural appearing. Discourage underground drainage.</p>
<p>Protection of Trees, Vegetation and Wildlife</p>	<p>LU12 Protect heritage specimens and groves of trees.</p>	<p>LU12.1a Require applicants to show location, size, and type of tree to be removed. Require compensation.</p>
	<p>LU13 Maintain diversity of vegetation types and wildlife habitats on hillsides and ridges.</p>	<p>LU13.1a Work to establish a management and maintenance program for open space lands.</p> <p>LU13.2b Protect acquisition of undeveloped lands with open space significance.</p> <p>LU13.2c The Design Review process will be used to identify the vegetation and wildlife habitats of a site.</p>

	<p>LU14 To ensure the long term protection remaining undeveloped lands that have been identified as having significant open space values.</p>	<p>LU14.1a Funding for acquisition of parcels with regional open space significance should be pursued by the Open Space District.</p> <p>LU14.1b Acquisition of parcels with local open space significance should be pursued.</p> <p>LU14.1c Portions of sites which contain open space resources shall be considered for preservation by clustering development.</p> <p>LU14.1d Identify parcels in this area which may be appropriate for acquisition as open space.</p>
	<p>LU.15 To protect wildlife trails through private property for access to water and food sources.</p>	<p>LU15.1a Any identified wildlife trails should be protected as part of a Design Review approval.</p>
<p>Reduction of Risk Associated with Natural Hazards</p>	<p>LU.16 Minimize hazards of natural and induced events.</p>	<p>LU16.1a May require the submission of geotechnical and hydrologic reports to assess risk.</p>
	<p>LU.17 Protect from danger to life and property caused by flooding.</p>	<p>LU17.1a The County will enforce the guidelines to development set forth in Program LU1.4d.</p> <p>LU17.2a The County will regulate development in the flood plain.</p> <p>LU17.3a The County Office of Emergency Services will work with residents to develop evacuation plans for flood-prone areas.</p>
	<p>LU.18 To ensure that needed facilities in a natural disaster are not located in the flood plain.</p>	<p>LU18.1a Inventory facilities within the Planning Area's identified 100-year flood plain.</p> <p>LU18.1b The County will review all proposed uses of land in the 100-year flood plain.</p>

<p>Ownership and Reuse of Publicly Owned Land and Facilities</p>	<p>LU.19 Allow some interim use of the publicly owned property to provide the public agency some financial return on its land holdings.</p>	<p>LU19.1a The County should meet with the School Districts and other public agencies to inform them of Community Plan policies.</p> <p>LU19.2a Public Works should not issue any new permits for roadside businesses.</p> <p>LU19.2b Rezone the Caltrans Corporation Yard to Public Facility (PF).</p> <p>LU19.2c Rezone parcels currently owned by the Flood Control District and used for flood control to "OA".</p> <p>LU19.2d The privately-owned parcel used for public assembly, TVIC, should be rezoned to OA (Open Area).</p> <p>LU19.2e When evaluating a use permit for the improvement clubs and schools the value to the community should be considered.</p>
	<p>LU.20 Stipulate the use or reuse of all School District lands in the event that sites are sold.</p>	<p>LU20.1a If public school sites are sold or redeveloped, the underlying residential zoning shall apply. Units should be clustered and senior or low-income housing are encouraged.</p> <p>LU20.2a Play fields shall be preserved for recreation use.</p>
<p>TAMALPAIS VALLEY</p>		
<p>Protection and Regulation of Parcels With Open Space Values</p>	<p>LU.21 Rezone properties secured as open space to an open space zone, and to identify parcels whose open space values need to be protected through regulation.</p>	<p>LU21.1a Parcel 52-100-12 has been dedicated to the GGNRA as permanent open space and should be rezoned to OA.</p> <p>LU21.1b Parcels along the Tennessee Valley Rd. have value as local open space. Special design regulations are needed to protect these values.</p> <p>LU21.1c Parcels along Shoreline Highway have both local and regional open space value. Uphill parcels are desirable for acquisition (Appendix L).</p>
<p>TAM JUNCTION</p>		
<p>Tam Junction Site Planning</p>	<p>LU.22 Establish site planning guidelines for the Tam Junction area.</p>	<p>LU22.1a Follow the site planning guidelines outlined in the Community Plan and Appendix H.</p>

East Side Land Use	LU.23 Protect Bothin Marsh and its environs, while providing for the limited commercial needs of the Tamalpais Planning Area.	LU23.1a Martin Brothers' Triangle has the highest priority for acquisition as open space.
		<p>LU23.1b Limitations on developing Martin Brothers' Triangle.</p> <p>LU23.1c Bothin Marsh will be protected and preserved by establishing a buffer zone.</p> <p>LU23.1e APN 52-181-05. One dwelling unit, or transfer one unit to contiguously held land. Seek acquisition of parcel by the Marin County Open Space District.</p> <p>LU23.1g APNs 52-052-05 through 09, 18, 22, 28, 29, 38, and 41. Mixed retail commercial and office occupancies to be developed in accordance with development and design guidelines.</p>
West Side Land Use	LU.24 Suitable for a variety of land uses, providing those uses will be developed as part of a cohesive village center.	<p>LU24.1a Residential units should be located over commercial uses. Freestanding units are not appropriate in the village core.</p> <p>LU24.1b Residential units should be incorporated in the design of commercial uses.</p> <p>LU24.1c Residential uses in the village core should include smaller, affordable units.</p> <p>LU23.1d Uses that do not contribute to the village core such as warehousing, or manufacturing, are not appropriate for the area.</p>
Open Space Acquisition	LU.25 Lands desirable for open space should be purchased.	No specific programs are identified.
Limitations on Commercial Expansion	LU.26 Prohibit any further commercial expansion in the Almonte and Miller Avenue commercial areas.	<p>LU26.1a Rezone commercial parcels in Almonte and Miller Ave. area to RMPC.</p> <p>LU26.1b Adopt an ordinance to amortize one legal, nonconforming use in Almonte: Graham Garage.</p>

Design Guidelines for Strip Commercial Areas	LU.27 Establish design guidelines for the Laurel-Poplar area, the Almonte Junction and Miller Avenue.	LU27.1a The commercial design guidelines specified in the community plan shall be applied.
HOMESTEAD VALLEY		
Residential Densities and Rezoning	LU.28 Rezone properties in the Homestead Valley neighborhood to achieve the objectives and policies of the Community Plan.	LU28.1a The larger undeveloped lots should be rezoned to conform to the minimum lot size required by the County's Slope Ordinance.
Protection and Enhancement of Existing Open Space Areas and Rezoning	LU.29 Complete Homestead Valley's trail system.	LU29.1a Parcels near the trail system should be required to provide a trail easement if development occurs. LU29.1b Continue the management agreement with the Homestead Valley Land Trust. LU29.1c Consider re-zoning the properties in the Camp Tamalpais Area to a planned district.
Fire and Treefall Hazard Reduction	LU.30 Reduce the potential of wildfire hazard posed by large stands of Eucalyptus and fields of broom.	LU30.1a The HVLTA will work with the residents and landowners to catalog and remove stands of eucalyptus which pose a risk to persons and property.
MUIR WOODS PARK		
Protection and Enhancement of Existing Open Space Areas	LU.31 Protect the significant local and regional open space values of the Muir Woods Park area.	LU31.1a APN 46-161-10. The community desires this site to remain open in appearance. LU31.1b Consider programs to acquire the forested undeveloped parcels in close proximity to Mount Tam State Park, Muir Woods National Monument and the lands of MMWD.
SHORELINE AREA		
Land Use and Development Intensity	LU.32 If acquisition is not possible, strict land use and development guidance shall be provided in the Community Plan.	LU32.1a A master plan which allows for the joint planning of multiple parcels is encouraged. All publicly held lands should be zoned OA. LU32.1b General commercial uses such as direct retail sales and services, should not be permitted in this area. LU32.1c Priority for the Krystal property is public acquisition. If the property is developed, the intensity of development shall be limited as shown in the community plan.

		<p>LU32.1d The priority for the Felton property is acquisition as open space. If the property is developed, the intensity of development shall be limited as shown in the community plan.</p> <p>LU32.1e The priority for the Landor property is public acquisition. Partial dedication for open space should be required if the site is developed. The intensity of development shall be limited as shown in the community plan.</p>
Development Guidelines for the Shoreline and Manzanita Areas	LU.33 Provide physical and visual public access to the shoreline of Richardson Bay while achieving design excellence for future projects.	<p>LU33.1a Future projects include an inventory of shoreline resources, a management plan, pedestrian and bicycle access, access for the physically impaired, and points of visual access. Such public access shall not adversely impact wetland habitats.</p> <p>LU33.1b Development should adhere to the design guidelines in the plan.</p> <p>LU33.1c Development should adhere to the circulation guidelines in the plan.</p> <p>LU33.1d Development should adhere to the landscape guidelines in the plan.</p> <p>LU33.1e Development should adhere to the Environmental Graphics and Lighting program.</p>
MANZANITA AREA		
Protection of Wetlands and Hillside Area and Maintenance of Community Image	LU.34 Regulate land use and provide development guidance for new development in the Manzanita area.	<p>LU34.1a Rezone a portion of APN 52-182-02 03, and 04, (zoned R-3) to RMPC with an F.A.R. limit of 0.35. Expansion of the motel developments may be permitted by Master Plan.</p> <p>LU34.1b When applications are processed for APNs 052-182-01, 02, 03, clustering shall be required. The hillside areas should be dedicated as open space.</p>
		<p>LU34.1c Parcel #52-211-01, and 52-231-01, 02, and 03 should be rezoned to RMPC-0.1. Additional commercial uses shall be discouraged.</p> <p>LU34.1d The first priority for the Caltrans Corporation Yard is acquisition as a commuter parking lot and alternative access for the Shoreline Office .</p>

TRANSPORTATION ELEMENT		
Promoting Alternative Modes of Transportation	<p>T.1 Develop a system to encourage the use of public transportation, pedestrian, and bike paths.</p>	<p>T1.1a Encourage usage of bus service by providing bus stops and shelters.</p> <p>T1.1b Continue to maintain local, regional, and recreation transit service.</p> <p>T1.1c Find additional public transportation routes and alternatives to private automobile access.</p> <p>T1.2a Continue dialogue with the appropriate agencies to reduce traffic to the parks.</p> <p>T1.2b Encourage the GGNRA and the State Parks to implement the recommendations to improve the efficiency of transporting visitors to the parks,.</p> <p>T1.2c Encourage the GGNRA and the State Parks Department to limit or restrict private automobile access to the recreation areas.</p> <p>T1.3a The County Open Space District should continue to develop and restore community trails and bike paths, where appropriate.</p> <p>T1.3b The Marin County Open Space District should prepare a pedestrian and bike trail plan.</p> <p>T1.3c Ensure proper trail maintenance, including controls for the use of trails by bikes, bicycles, and equestrians.</p> <p>T1.3d Examine the possibility of providing more efficient pedestrian and bicycle routes in the Tam Junction area, particularly around Coyote Creek.</p> <p>T1.3e The School District should consider setting up a school advisory committee as described in the State Traffic Manual.</p> <p>T1.3f The Golden Gate Bridge Highway and Transportation District should install bicycle racks at all bus stops.</p>

		<p>T1.3g Bike trails should be indicated on a map, conform to County standards, and when possible should have a separate right-of-way.</p> <p>T1.3h During development review, require installation of bus shelters and bicycle racks at bus stops.</p> <p>T1.3i Evaluate the feasibility of installing a pedestrian activated signal at Shoreline Highway and Pine Hill Road.</p>
<p>Existing and Future Traffic Congestion</p>	<p>T.2: Improve traffic circulation along Shoreline Highway/State Route 1 while protecting, to the greatest extent possible, the values of the natural environment.</p>	<p>T2.1a Improve Shoreline Highway from Coyote Creek to the signalized intersection at Almonte Boulevard.</p> <p>T2.1b Consolidate driveways along Shoreline Highway/State Route 1.</p> <p>T2.2a Improve the Shoreline Highway/State Route 1 from Tam Junction to Flamingo Road.</p> <p>T2.2b Land use plans for the Cala Foods property shall require access points to be located a minimum of 50 feet from the Shoreline Highway/State Route 1 intersection.</p> <p>T2.2c Signage and landscaping at the intersection of Shoreline Highway and Flamingo Road should be improved.</p> <p>T2.3a The County shall work with Caltrans to widen Shoreline Highway/State Route 1 to include center turn lanes and shoulder improvements.</p> <p>T2.3b The County shall install a landscaped median along Shoreline Highway at Ross, Spruce and Ash streets.</p> <p>T2.4a Design Review and master plan applications shall be reviewed to determine if there is an alternative for access other than connected directly to Shoreline Highway.</p> <p>T2.4b The County, to the extent possible, shall prohibit additional access directly onto Shoreline Highway/State Route 1.</p>

		<p>T2.5b Under no circumstances shall Shoreline Highway/State Route 1 between Flamingo Road and Loring Way be widened to four lanes.</p> <p>T2.6a. A planning study should be undertaken which carefully examines Shoreline Highway/State Route 1 from the U.S. Highway 101 interchange to Tam Junction.</p>
Level of Service Standard for Intersections	T.3 To establish a LOS standard for each of the three major intersections.	<p>T3.1a The County shall work with Caltrans to improve the Tam Junction intersection.</p> <p>T3.1b Widen Shoreline Highway as described in Program T2.1a.</p> <p>T3.2a Implement the installation of a traffic control signal at the Manzanita intersection.</p> <p>T3.2b Add a left turn lane to the south approach of U.S. Highway 101 and make the southbound on-ramp traffic split off sooner.</p> <p>T3.3a Provide access to the Shoreline area over a new road which would form the north leg of the Manzanita intersection.</p> <p>T3.3b Work with Caltrans to widen Shoreline Highway/State Route 1 from the U.S. Highway 101 northbound ramps to the Manzanita intersection.</p> <p>T3.4a Develop a traffic mitigation ordinance designed to maintain or achieve LOS D at all intersections and assist in providing funds to undertake improvements.</p>
Relationship of Development Intensity to Roadway Capacity	T.4 Ensure that roadway improvements conform to the policies and programs set forth in this plan.	T4.1a Traffic studies will be required development could generate traffic with the potential to degrade the level of service.
Roadway Extensions	T.5 Oppose second access from Marin City to Tennessee Valley Road.	<p>T5.1a Limit improvements to Tennessee Valley Rd. to those needed for safety, and maintain Tennessee Valley Rd. in its current configuration to the extent possible.</p> <p>T5.1b Extend the left turn lane on Shoreline Highway at Tennessee Valley Road if space is available.</p>

<p>Roadway Design and Character</p>	<p>T.6 Retain the character of the area by assuring that roadway construction and improvements will have a low impact on the environment.</p>	<p>T6.1a Support retention of the semi-rural character by discouraging street lights, concrete sidewalks, curbs, and gutters.</p> <p>T6.2a Keep road widths and intersection designs to the minimum required.</p> <p>T6.2b Retain existing unimproved watercourses in their natural state. Culverts are to be discouraged.</p> <p>T6.3a Eucalyptus foliage on the south side of Shoreline Highway should be cut back.</p> <p>T6.3b Redesign the intersection at Four Corners (junction at Sequoia Valley Rd).</p> <p>T6.4a. Require developers to post a bond proportional to the size of the project.</p>
<p>Control of Visitor and Recreation Traffic</p>	<p>T.7 Limit the impacts of visitor traffic on the Planning Area.</p>	<p>T7.1a Implement T1.2a-c.</p> <p>T7.2a Install signage indicating alternate access routes to west Marin are available.</p> <p>T7.2b Locate a sign at the Manzanita intersection indicating when beach parking lots are full.</p>
<p>Parking</p>	<p>T.8 To improve off-street parking in hillside areas and historic subdivisions.</p>	<p>T8.1a Where on-street parking is limited, create small landscaped parking sites.</p> <p>T8.1b Require all new parking lots to have landscape plans that include trees to break up visual impact.</p> <p>T8.1c Fund an assessment district to provide parking patrols should be considered.</p> <p>T8.2a Require all new residential construction or remodels, provide additional parking if served by sub-standard streets.</p>

	<p>T.9 Maintain and improve commuter parking facilities in the Planning Area.</p>	<p>T9.1a Existing commuter facilities shall be preserved and new lots acquired when available.</p> <p>T9.1b Adopt a monitoring and enforcement program to limit long-term parking in commuter lots.</p> <p>T9.1c The County shall not lease public lands to private developers for private parking use.</p>
	<p>T.10 To ensure that non-residential development has adequate parking, do not allow the leasing of land for parking to increase building floor area for a parcel, or to meet on-site parking requirements.</p>	<p>T10.1a The County shall not permit the use of land leased for parking or adjacent street parking in calculating allowable floor space or in meeting on-site parking requirements.</p>
<p>Road Access to Hillside and Ridge Properties</p>	<p>T.11 Require adequate road access to new residential development in hillside and ridge areas and within historic subdivisions.</p>	<p>T11.1a Continue to enforce the Paper Street Ordinance and Title 24 development standards for the proposed improvement of paper streets.</p> <p>T11.1b Require the applicant to submit a study of the existing or potential road conditions and the recommended improvements necessary as part of a complete application.</p> <p>T11.1c Landowners or developers may be required to submit fees or provide easements and improvements recommended in the roadway study.</p> <p>T11.2a Set up a task force to investigate ways to enforce parking standards on private roads.</p>
<p>Trail System and Community Accessway Preservation and Maintenance</p>	<p>T.12 Preserve community accessways and provide trails in the Tamalpais Planning Area.</p>	<p>T12.1a Consider acceptance of offers of dedication to obtain the accessways, parcel separators, easements and/or alleys.</p> <p>T12.1b Evaluate proposed abandonments of easements, alleys, and pedestrian accessways with respect to the policies described in this Community Plan and the Countywide Trails plan.</p> <p>T12.2a Require dedication of trail rights-of-way at the time of development if the Countywide trails map indicates there is a trail on the property.</p>

		<p>T12.2b Connections to bus stops, shopping areas, and recreation areas should be the highest priority for allocation of improvement funding.</p> <p>T12.2c Property owners should be aware that they may not construct improvements which preclude access within a public access easement.</p>
Roadway and Trail Signing	T.13 Improve automobile, bicyclist and pedestrian safety by providing signs which promote safety.	<p>T13.1a Install and maintain fog stripes and reflectors on roads in the foggy upper elevations.</p> <p>T13.1b Encourage Caltrans to install a sign at Shoreline Hwy. and Loring Ave. that indicates that the curve is dangerous.</p> <p>T13.1c Encourage Caltrans to post and maintain bike paths with "No Parking" signs.</p> <p>T13.1d The County, the TCSD, the GGNRA or other responsible agencies shall install signs to mark trail heads.</p> <p>T13.1e Trail entries should be signed in a uniform "rustic style."</p> <p>T13.1f Trail entries should include bollards or blind gates which prohibit motor vehicle and motor bike access.</p>
Funding Transportation System Improvements	T.14 Provide a funding program which would assist in funding the transportation improvements	<p>T14.1a Approve a Traffic Mitigation Fee Ordinance which requires a traffic mitigation fee based on the trips which would be generated.</p> <p>T14.1b The improvements which shall be funded Traffic Mitigation Fee Ordinance are described in the Plan.</p> <p>T14.1c The Fee Ordinance shall include calculations of the rate be paid by each development.</p>
PUBLIC FACILITIES AND SERVICES		
Adequacy of Public Facilities and Services to Meet Needs of New Development	PS.1 Adequately and efficiently provide public services to residents, and new development.	<p>PS1.1a Environmental review on development which requires expansion or improvement of public facilities.</p> <p>PS1.1b Require development to pay its fair share of the infrastructure.</p>

		PS1.1c Submit letters of verification from public service providers that capacity is available, prior to approval of the development application.
Maintaining an Adequate Long Term Supply of Water in the Community	PS.2 Work with the Marin Municipal Water District to insure a long-term adequate supply of high quality drinking water for the community.	PS2.1a Encourage MMWD to pass onto new development its fair share of the costs of water supplies improvements. PS2.1b The community has a preference limited water supplies be allocated for residential uses. PS2.1c Encourage MMWD to maintain the current high quality of its water.
Individual Waste Disposal Systems in Muir Woods Park	PS.3 Retain individual waste disposal systems in the Muir Woods Park neighborhood.	PS3.1a Permit existing dwellings with failing or marginal septic systems to; a) Use alternative methods of sewage disposal, b) Join a contracted septic system maintenance program, c) Hook up to an existing public sewerage system, where feasible, without forcing neighborhood annexation.
Providing an Adequate Level of Fire Protection	PS.4 Maintain an adequate level of fire protection for all of the open space lands, residential neighborhoods, and commercial areas.	PS4.1a The Tamalpais Fire Protection District should recruit additional volunteers, train personnel to provide the best emergency medical care, and purchase new equipment. PS4.1b Identify areas where access for emergency vehicles is impaired, and take steps to rectify. PS4.1c The Multi-Hazard Response Plan should be reviewed and revised where necessary. PS4.2a The Water District should evaluate the adequacy of the existing water pressure in the Planning Area. PS4.2b New development should be required to replace existing hydrants to meet modern fire flow requirements.

<p>The Provision of Adequate Police Protection in the Community</p>	<p>PS.5 To improve Sheriff service in the Tamalpais Planning Area.</p>	<p>PS5.1a Maintain a Sheriff substation in southern Marin, and assign officers in proportion to the population.</p> <p>PS5.1b The County should recognize that increasing population without increased Sheriff's service will result in a reduced level of service in the area.</p> <p>PS5.1c Speed limits shall be enforced throughout the Planning Area.</p> <p>PS5.1d Parking enforcement in the Planning Area on non-County maintained roads.</p>
<p>The Effects of New Development on Stream Erosion and Flooding</p>	<p>PS.6 Limit the impacts of new development on stream erosion and down stream flooding.</p>	<p>PS6.1a Evaluate the impact of hillside development for increasing down slope flooding and erosion, as well as silting of the marshlands and flood control channels.</p> <p>PS6.1b Establish a mitigation fee to contribute toward improvements to reduce storm drainage impacts.</p> <p>PS6.1c Prepare a new Master Drainage Plan and investigate alternative methods of flood control in the Crest Marin area.</p>
	<p>PS.7 Establish a maintenance program for existing flood control projects.</p>	<p>PS7.1a Establish a maintenance program for Tam Valley and implement the balance of the Tamalpais Valley Master Drainage Plan.</p> <p>PS7.1b Establish a program define the Reed Creek flood plain.</p>
<p>Resolution of Jurisdictional and Public Service Boundary Disputes</p>	<p>SI.1 Resolve jurisdictional and public service boundary conflicts that result in inefficient provision of services.</p>	<p>SI1.1a Encourage the City of Mill Valley to determine an appropriate City boundary line along Miller Avenue.</p> <p>SI1.1b Encourage the City of Mill Valley to determine an appropriate City boundary line along Edgewood Road.</p> <p>SI1.1c Encourage the City of Mill Valley to determine an appropriate City boundary line adjacent to the Muir Woods Park neighborhood</p> <p>SI2.2a Petition the City of Mill Valley to adjust the boundaries of its Urban Service Area and Sphere of Influence.</p>

APPENDIX B.

**RESIDENTIAL FLOOR AREA RATIO FORMULA
FOR SUBSTANDARD OR LOTS ON > 25% SLOPE**

**ADJUSTED MAXIMUM FLOOR AREA FOR SUBSTANDARD*
LOTS OR LOTS WITH A SLOPE OF 25% OR GREATER**

Lot Size		Maximum Adjusted**	Maximum Adjusted
(Sq. Ft.)	(Acres)	Floor Area (Sq. Ft.)	Floor Area Ratio
3,000	0.07	900	0.30
3,500	0.08	1,050	0.30
4,000	0.09	1,200	0.30
4,500	0.10	1,350	0.30
5,000	0.11	1,500	0.30
5,500	0.13	1,650	0.30
6,000	0.14	1,800	0.30
6,500	0.15	1,950	0.30
7,000	0.16	2,100	0.30
7,500	0.17	2,250	0.30
8,000	0.18	2,400	0.30
8,500	0.22	2,550	0.30
9,000	0.21	2,700	0.30
9,500	0.22	2,850	0.30
10,000	0.23	3,000	0.30
11,000	0.25	3,050	0.28
12,000	0.28	3,100	0.26
13,000	0.30	3,150	0.24
14,000	0.32	3,200	0.23
15,000	0.34	3,250	0.22
17,500	0.40	3,375	0.19
20,000	0.46	3,500	0.18
25,000	0.57	3,750	0.15
30,000	0.69	4,000	0.13
35,000	0.80	4,250	0.12
40,000	0.92	4,500	0.11
45,000	1.03	4,750	0.11
50,000	1.15	5,000	0.10
55,000	1.26	5,250	0.10
60,000	1.38	5,500	0.09
65,000	1.49	5,750	0.09
70,000	1.61	6,000	0.09
75,000	1.72	6,250	0.08
80,000	1.84	6,500	0.08
85,000	1.95	6,750	0.08
90,000	2.07	7,000	0.08

* *A substandard lot is any legal lot of record that has a land area less than required by current zoning code for the district in which it is located.*

** *Adjusted floor area includes garage space (greater than 400 - 480 sq. ft.), accessory buildings, second units and space which could easily be converted to additional living space. The maximum adjusted house size for lots 90,000 square feet or greater shall not exceed 7,000 square feet. Please see Program LU1.4d for a more detailed description.*

APPENDIX C.

BOTHIN MARSH BUFFER ZONE

BOTHIN MARSH BUFFER ZONE

In order to preserve Bothin Marsh, one of the few remaining marshlands in the northern sections of San Francisco Bay, a buffer zone should be provided between the marsh and nearby development.

The purpose of this section is to provide a general framework for the creation of such a buffer zone, rather than to define the precise size or necessary area. The following is a brief description of buffer zone purposes, elements, adjacency considerations, and general design guidelines. A precise design for the buffer zone should be determined as part of a marsh preservation plan and would result from a careful analysis of wildlife habitats, soil conditions, pollution, etc. Or a definitive buffer zone design might result as a part of specific development planning for contiguous upland areas. In the second case, buffer zone size and design characteristics would be determined in response to specific development proposals. This would be necessary in order to control the potential impacts of a development project on the marsh due to the location of critical elements in the development plan; for example - the proximity and amount of parking.

Purposes

Buffer zone purposes include: 1) Control human intrusion, 2) Control animal intrusion, 3) Limit pollution, 4) Provide limited viewing points within the buffer zone, 5) Preserve views from the marsh, and 6) Modulate views of the marsh.

Creation of the buffer zone should not be solely dependent on size or distance. The effect of buffering or separating activities can also be achieved with the use of vegetation, earth berms or mounds, changes of level, moats¹ and man-made barriers such as fencing or bollards. These variables might be combined in different ways to achieve the same ends.

Adjacent Uses

The size of the marsh buffer zone may vary relative to the adjacent human uses, because different uses would create different kinds and types of potential intrusions into marsh life.

Potential adjacencies include:

1. Roadway - Buffer Zone - Marshland
2. Parking - Buffer Zone - Marshland
3. Single Family Residence - Buffer Zone - Marshland
4. Apartments - Buffer Zone - Marshland
5. Existing Commercial - Buffer Zone - Marshland
6. Office, Commercial and Residential - Buffer Zone - Marshland

Buffer Zone Design Guidelines

The following design guidelines are intended to set a general framework for the design of a buffer zone between Bothin Marsh and any adjacent development. Placement of certain design elements, such as walkways and bikeways, should be carefully considered in order to respect and preserve

¹Suggested by Dr. Joel Gustafson, January 17, 1980.

certain sensitive areas within the marsh yet permit visual access by observers.

General Design Elements

1. Access

- a. Direct access to the marsh surface (foot traffic or bicycling) is inappropriate and should be prohibited; "in some cases . . . a boardwalk over a portion of the marsh may be appropriate."
- b. Access to the boundaries of the marsh should be restricted to point rather than continuous access, "in order to mitigate the adverse impact of human intrusion on wildlife resources, especially the more timid species".²
- c. Access to the buffer zone should be provided by walkways or trails and should be connected to the nearest public thoroughfare, in accordance with BCDC's public access requirements.³

2. Paths

In addition to paths directly to observation points, a path or trail might be provided through the buffer zone. This path would be for walking or jogging and would be located away from the marsh boundaries.

3. Bikeway

Consideration should be given to provision of a bikeway through the buffer zone. Such a bikeway should also be located away from the marsh boundaries.

4. Other Improvements

"Provide minimal improvements such as trash containers and signs which identify the area and interpret the resources."⁴

5. Debris

"Remove unnatural debris from shoreline access sites."⁵

6. Contours and Plant Materials

"Maintain, enhance, or complement the site's natural contours, features and plant materials."⁶

²"Public Access Supplement", The Bay Shoreline Element. San Francisco Bay Conservation and Development Commission, pages 6-9.

³ Ibid

⁴ Ibid

⁵ Ibid

⁶ Ibid

7. Continuing Maintenance

Provide funds for continuing maintenance of the buffer zone.

8. On Site Parking

Provide a small on-site public parking area which is kept away from the marsh boundaries and is adequately screened.⁷

Location

Within the buffer zone the major design elements described above should be located relative to the marsh, the roadway and to each other as follows:

Roadway - Parking - Bikeway - Thru-Site Pathway -

Pathway to Observation Points - Observation Points - Vegetation Buffer -

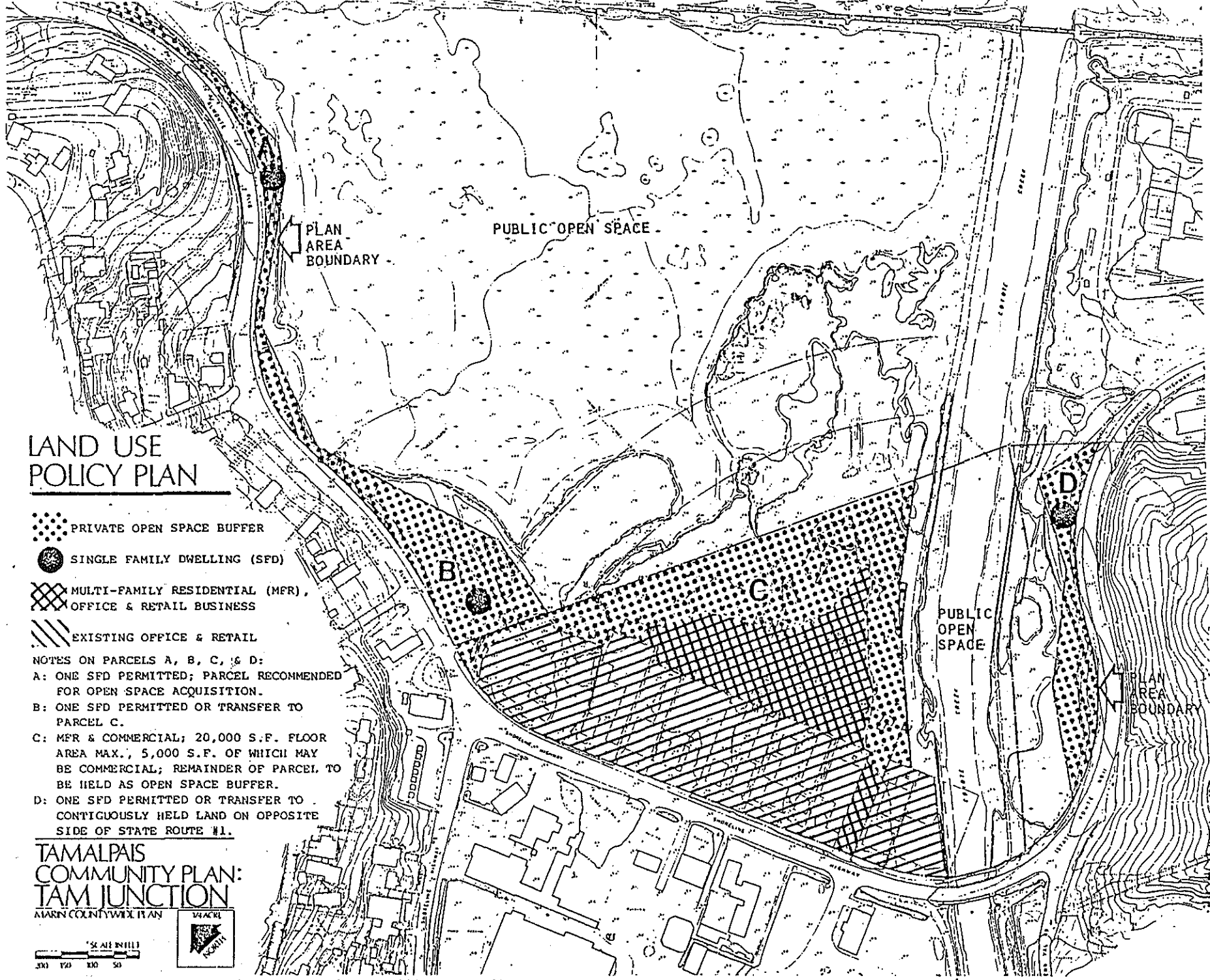
Marsh

Barrier Free Design





Design buffer zone facilities for the physically handicapped to the maximum extent possible.

⁷ Ibid.

⁸ Ibid.

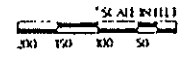


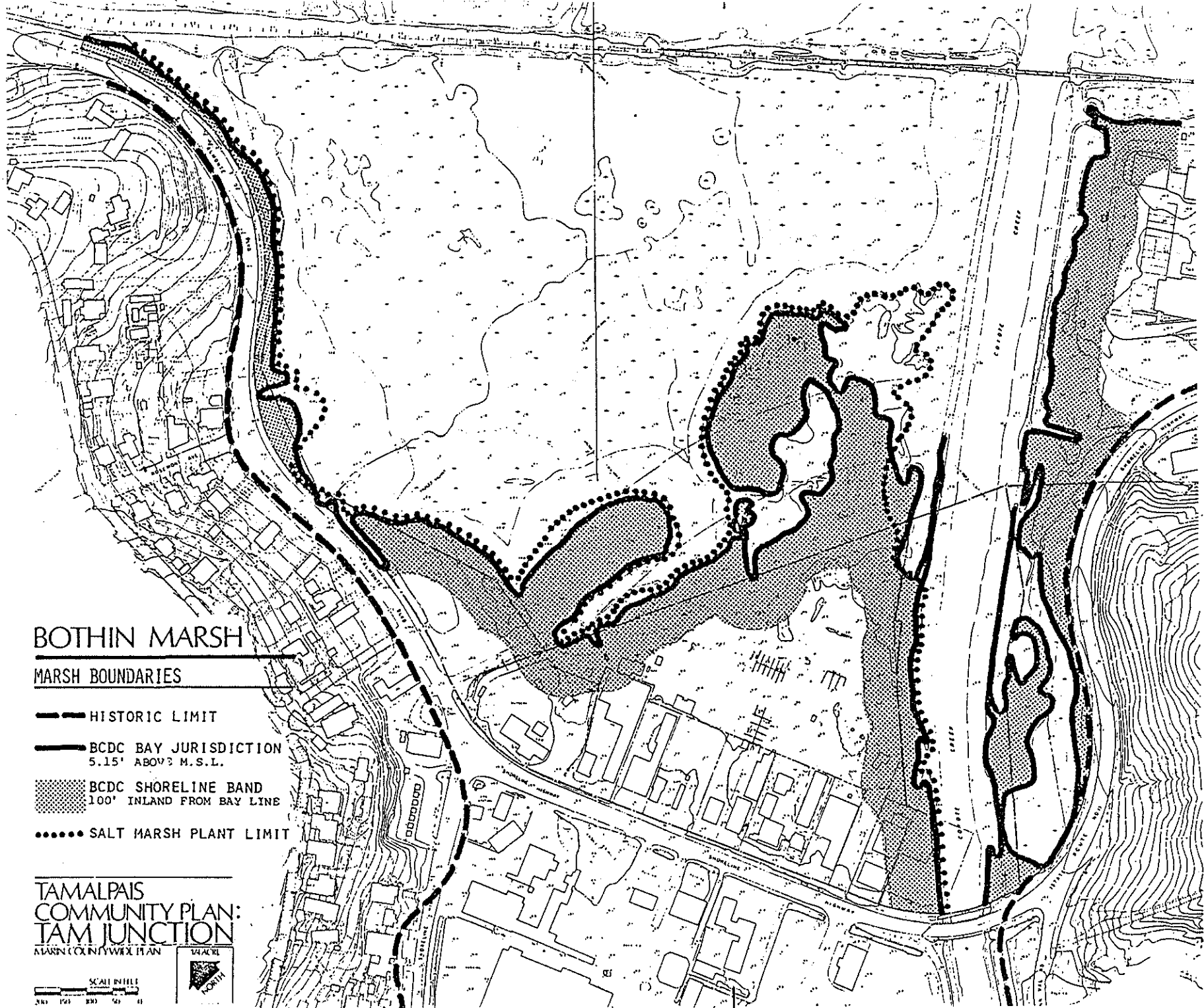
LAND USE POLICY PLAN

-  PRIVATE OPEN SPACE BUFFER
-  SINGLE FAMILY DWELLING (SFD)
-  MULTI-FAMILY RESIDENTIAL (MFR), OFFICE & RETAIL BUSINESS
-  EXISTING OFFICE & RETAIL

- NOTES ON PARCELS A, B, C, & D:
- A: ONE SFD PERMITTED; PARCEL RECOMMENDED FOR OPEN SPACE ACQUISITION.
 - B: ONE SFD PERMITTED OR TRANSFER TO PARCEL C.
 - C: MFR & COMMERCIAL; 20,000 S.F. FLOOR AREA MAX., 5,000 S.F. OF WHICH MAY BE COMMERCIAL; REMAINDER OF PARCEL TO BE HELD AS OPEN SPACE BUFFER.
 - D: ONE SFD PERMITTED OR TRANSFER TO CONTIGUOUSLY HELD LAND ON OPPOSITE SIDE OF STATE ROUTE #1.





TAMALPAIS
COMMUNITY PLAN:
TAM JUNCTION
MARIAN COUNTY WELLS PLAN



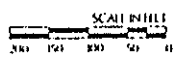


BOTHIN MARSH

MARSH BOUNDARIES

-  HISTORIC LIMIT
-  BCDC BAY JURISDICTION
5.15' ABOVE M.S.L.
-  BCDC SHORELINE BAND
100' INLAND FROM BAY LINE
-  SALT MARSH PLANT LIMIT

TAMALPAIS
COMMUNITY PLAN:
TAM JUNCTION
MARIN COUNTY WEA PLAN



APPENDIX D.
SHORELINE AND MANZANITA AREAS DEVELOPMENT GUIDANCE

**TAM AREA COMMUNITY PLAN
SHORELINE AND MANZANITA AREAS**

DEVELOPMENT GUIDANCE

February 20, 1989

A. USE OF DEVELOPMENT GUIDES

Purpose:

The purpose of these interim design guidelines and development standards is to aid community residents, potential developers and decision makers in the preparation, evaluation and approval or disapproval of development proposals in the Tam Valley planning area. These guidelines and standards are intended to be specific enough to allow project sponsors to determine, in advance, the current planning policies for the Tam Valley planning area while allowing sufficient design flexibility, and to provide the community with adequate standards by which to evaluate a proposed project. These interim guidelines are intended to supplement the Marin County-Wide Plan, the Richardson Bay Special Area Plan, BCDC's Bay Plan and Title 22 and 24 of the Marin County Code which are incorporated in these guidelines by reference.

How to Use These Guidelines:

These design guides and development standards, as part of the Community Plan, should be made available to potential project sponsors before they initiate the planning and design process for their projects. As the proposed project evolves, these guides should be reviewed periodically by the planning staff and the project proponent to assure that the community's goals are being met. Upon completion of the development proposal, the planning staff and the Tam Area Design Review Board should use these guides to evaluate the proposed development and offer recommendations for adjustments, if necessary.

These guides should be reviewed periodically by the community for currency and revised as necessary. These interim guidelines, while guiding any development proposals which are processed prior to adoption of the Community Plan, are not intended to be complete at this time and are subject to the final policies and recommendations of the Tam Area Community Plan.

Intent:

The intent of these guidelines is:

- to provide project proponents with measurable limits for height, building mass, building setbacks from property lines and special site features, such as the shoreline, amount of landscaping, parking requirements, including screening of vehicles and other site amenity requirements.
- to provide a skeletal design framework by which project planners and designers can develop a site Master Plan or Specific Plan, including proposed architectural elements.
- to provide the community with high quality site planning, architecture, urban design and landscape design that will reinforce and enhance the quality of life preferred by the community as expressed by the Goals and Objectives including in the Community Plan.
- to allow some design flexibility by providing design guidance while encouraging design excellence. If a superior design that accomplishes the Community's Goals and Objectives can be achieved that does not adhere exactly to the measurable guidelines expressed here, that design may be favorably considered. The Tam Design Review Board shall be relied upon to guide interpretation of the guidelines and to help review the alternative designs and plans submitted to the County Planning Department. The Land Development Section of the Public Works Department shall determine the site finished floor elevation.

B. DEVELOPMENT GUIDES

1. Use

Consistent with the current zoning designation for the Shoreline area, BFC-R-C-R (Bayfront Conservation/Resort Commercial - Recreational Planned District); R-C-R (Resort Commercial - Recreational Planned District); BFC-C-P (Bayfront Conservation/Planned Commercial); and C-P (Planned Commercial) requires a master plan. All publicly held lands should be zoned OA (Open Space). A master plan which allows for the joint planning effort of multiple parcels is encouraged.

The primary purpose of this designation is to create and/or protect resort and resort oriented recreation facilities by controlling structure height and lot coverage and by permitting and enhancing public access to the shoreline recreation resources in the area.

General Commercial uses are not permitted in this area, under the current zoning designation.

Based on the current and preferred future use of the area, the zoning designation for the Manzanita Area is Resort - Commercial - Recreational (RCR). Expansion of the existing motel developments is allowed, with controls. New development of the slopes and hillside areas west of the Shoreline Highway is not allowed.

2. Public Access

Intent:

The intent of these guidelines is to provide, where possible, physical and visual public access to the shoreline of Richardson Bay along the Shoreline and Manzanita areas while achieving design excellence for proposed projects in order to maintain or improve the quality of life in the area.

All future development plans shall:

- Include an inventory of the existing shoreline resources found within the boundaries of the proposed project.
- Provide a formal management plan addressing biological concerns associated with the shoreline/marsh area adjacent to the proposed development. The plan shall include formal consultation with the Army Corps of Engineers, National Marine Fisheries Services, U.S. Fish and Wildlife Service, San Francisco Bay Conservation and Development Commission and California Department of Fish and Game.
- Provide public access to and along the shoreline open space by means of pedestrian and bicycle pathways which are clearly marked and separated from each other. The purpose of this guideline is to allow continuous access for the public along the shoreline and between structures.
- Provide access and amenities to the shoreline open space area for the physically handicapped.
- Provide, in addition to physical access, areas or points of visual access to the shoreline open space from buildings and between buildings. The purpose is to eliminate a wall-like effect along the shoreline.

3. Design Character

Intent:

The intent of this section of the guidelines is to provide a design framework for the buildings and other structures proposed for development in the Shoreline and Manzanita Areas. In order to upgrade the visual character of the entrance to the Tam Valley/Mill Valley Area, provide for maximum visual access to the bay, protect the natural environment of the bay, and enhance recreation use, final designs shall conform within the following limits and shall be subject to approval by the Tam Design Review Board.

Future development proposed for this area shall:

- Conform to the zoning designation for the area.
- Include adequate on-site parking for facilities as required by the current zoning ordinance.
- Be restricted to the land portion of any site, excluding any lands below the high tide lines.
- Prohibit any new development on the hillside areas west of Shoreline Highway in the Manzanita Area.
- Be designed so that the proportions of the development are compatible with other development in the area.
- Be oriented toward the shoreline and Bay, maximizing views and vistas from buildings and from pedestrian access and other sites and structures, such as Almonte Hill or Richardson Bay Bridge.
- Establish an area of open space, at least 100 feet in width, from the edge of the shoreline, as measured from the high water line, to the first horizontal element of the proposed project to protect the views and natural environment of the existing shoreline.
- For the Shoreline Area, any proposed development be limited in height to 25 feet for the Felton and Landor properties and 35 feet for the Krystal property. The height limitation for the Krystal property assumes that all parking for the proposed development will be in a structure. These limits indicate the maximum height of any building or structure, including all rooftop mechanical equipment, when measured from the lowest finished floor elevation of 8' MSL as specified by the Land Development Section of the Department of Public Works.

- For the Manzanita Area be limited to 37 feet on the west side of Shoreline Highway and 30 feet on the east side for all new construction. These limits indicate the maximum height of any new building or structure, including all rooftop mechanical equipment of when measured from the lowest finished floor elevation of 8' MSL as specified by the Land Development Section of the Department of Public Works.

AND

For the Manzanita Area, all new development shall be limited to three (3) stories on the west side of Shoreline Highway and two (2) stories on the east side.

- Use building construction and design techniques that create structures that are equally attractive on all sides, rather than placing an emphasis on the front elevation of the structure and neglecting or downgrading the aesthetic appeal of the side and rear elevations of the structure.
- Be designed so that the longest dimension of any building is perpendicular to the shoreline so that views can be obtained through the development to the Bay, unless it can be shown that the configuration of a particular parcel was in conflict with the intent of this guideline to provide visual access.
- Provide a minimum landscaped and/or open space area of 30 percent of the proposed project site for the Felton and Landor properties and 50 percent for the Krystal property, exclusive of any area bayside of the high water line.
- Include structure design and site furnishings which are compatible with the style and character of existing development.
- Provide screened or enclosed areas for waste collection and storage equipment (dumpsters, trash receptacles, recycling facilities).
- Include roof surface materials, texture and reflectivity that have been considered for their effects on the views of other adjacent sites and structures.
- Include rooftop mechanical equipment, vents and ducts that are completely screened, covered and installed in a manner which prohibits observation from other sites and structures.

- Include, if part of the project design, rooftop solar collectors, skylights and other potentially reflective rooftop building elements that are designed and installed in a manner which prevents reflected glare and obstruction of views from other sites and structures.
- Not allow rooftop radio, TV and microwave antennas and towers.
- Allow a maximum floor area (FAR) of 0.4 of the dry land area (excluding therefrom any wetlands and any submerged areas) for the Felton Property, 0.3 for the Landor property and 0.35 for the Krystal property.

4. Circulation

Intent:

The intent of this section is to improve the existing circulation conditions for vehicles, pedestrians, cyclists and equestrians in the Shoreline and Manzanita areas. All proposed development must include provisions which contribute to this intent. The primary purpose of these guidelines is to enhance the quality of life for the residents of the Tam Valley and Mill Valley area.

Future development plans shall:

- Restrict vehicular circulation in the Shoreline Area to the Shoreline Highway side of the development.
- Include separate and well marked bicycle and pedestrian pathways, which are linked to the shoreline path system connecting Mill Valley with Sausalito.
- Include convenient amenities for and access to public transit.
- Locate and conceal service access and service areas away from the shoreline side of the structures in the Shoreline Area.
- Provide landscaped parking areas between Shoreline Highway and the proposed structures, and away from the shoreline open space.
- Include design elements, such as plant materials, landscaped berms, to screen the parking areas from view from Shoreline Highway and the shoreline open space area in both the Shoreline and Manzanita Area.
- Include parking spaces, ramps and paths which accommodate physically handicapped individuals.

- Include no signaled intersections.
- Provide only on-site parking as required by current County standards.
- Include a landscape plan for the Caltrans parcels between Shoreline Highway and the motel, office and gas station east of the roadway. The intent of this landscape plan is to enhance the gateway image of the community by removing the opportunity for vehicular parking of any duration on the undeveloped areas adjacent to Shoreline Highway and to increase the amount of landscape area.
- Include an implementation program that establishes an landscape assessment district to develop and maintain the landscaped areas adjacent to Shoreline Highway in the Manzanita Area.
- Prohibit parking of any vehicle adjacent to the road surface of any arterial roadway in the Manzanita Area unless in designated parking areas.
- Prohibit any temporary commercial use along arterial roadways and their right of way.

5. Landscaping

Intent:

The intent of these guidelines is to enhance the visual quality of the Manzanita and Shorelines Areas and to integrate these areas with Tam Junction and the rest of the planning area. Any new development proposals should contribute to the visual quality of the area.

Future development proposals shall:

- Provide a landscaping plan which includes drought tolerant native plants or compatible species as the predominate feature.
- In the Shoreline Area provide a landscaping plan that has two primary concepts, one relating to the shoreline open space side of the development, and the other relating to the entry and parking area on the Shoreline Highway side of the development.

- Provide a landscaping plan and maintenance schedule which preserves and enhances the views and vistas to the Bay and to Mt. Tamalpais from the public areas, specifically the pathways.
- Include a landscaping plan that employs a mix of trees, shrubs, ground covers and turf where appropriate. The plant palette should be relatively limited and applied in groupings of similar species rather than a few plants of many different species planted together. The use of water conserving plantings, such as California natives and drought tolerant trees, shrubs and turf is encouraged. Trees should be planted for shade, screening and interest.
- Include a landscaping plan that is designed, installed and maintained in accordance with xeriscape principles. The goals of xeriscape are to provide a low maintenance, water conserving, functional and attractive landscape.
- Provide live plant materials in all landscaped areas. The use of gravel, colored rock, bark and other similar materials are not acceptable as a sole ground cover material.
- Provide automatic irrigation for all landscaped areas. Plants should be watered and maintained on a regular basis. Irrigation systems should be designed so as not to overspray walls, buildings or parking areas. The use of water conserving systems such as drip irrigation for shrub and tree planting is encouraged.
- Protect the solar access of property owners from encroachment by any structures or vegetation in parks, medians, parkways and drainageways.
- Include a palette of plant materials which are used to screen parking and service areas, including the Caltrans Manzanita Station.
- Include plant materials which are suitable for wind and sun protection.
- Include perimeter landscaped areas around the parking areas which are at least 10 feet in width, excluding internal roadways in addition to landscaping within the parking areas.
- Include a landscaped setback or roadside buffer area of at least 10 feet, similar to the setback condition exhibited by the Fountain Motel.
- Include a landscaped area between the property line and the Caltrans roadway surface.

- Provide a plan for placing all utility systems underground.
- Locate pad-mounted transformers, utility connections, meter boxes, mailboxes and other free standing structures as an integrated part of the site plan, to be concealed or screened to the maximum extent possible.
- Include paving materials and paving pattern design which is compatible with the building architecture of the proposed development and the adjacent development.

6. Environmental Graphics

Intent:

The intent of this section is to develop a program of graphics, including signs, for the Shoreline and Manzanita Area which will enhance the architectural and landscape design of the area and to improve the visual quality of the area. Environmental graphics should compliment the design of the buildings and other structures while providing information, and should not compete with the design quality of the architecture.

Future development plans shall:

- Incorporate all the requirements of Title 22.69 referring to the County sign requirements.
- Include user announcement signs which are integral with the building facade. No signs shall be perpendicular to the buildings.
- Include monument signs which are low and reflect the architectural character of the buildings.
- Include no signs which are lighted from within.
- Not include corporate identity graphic styles, logo type, colors or other devices that are inconsistent with the Tam Area Plan.
- Not include billboards, marquee signs, or decorative flags.
- Not include temporary "sandwich" style sign boards.

- Include a comprehensive plan for all signs, including building names and numbers, street signs, directional signs, informational signs, directory signs and other graphic devices, including emergency information signs, which relate to the proposed development. This comprehensive plan shall include type face style, height, depth and width of all letters, numbers, words or symbols, colors, materials, textures, and methods of application. Lighting techniques shall also be specified.
- Not include lighted signs that appear to move or cause motion to be perceived.
- Include a sign color palette which is compatible with the design goals of the Tam Area.
- Include a visitor-related sign program which provides location information, turning and stopping instructions, parking instructions, public transit information and pedestrian and bicycle movement instructions.

7. Lighting

Intent:

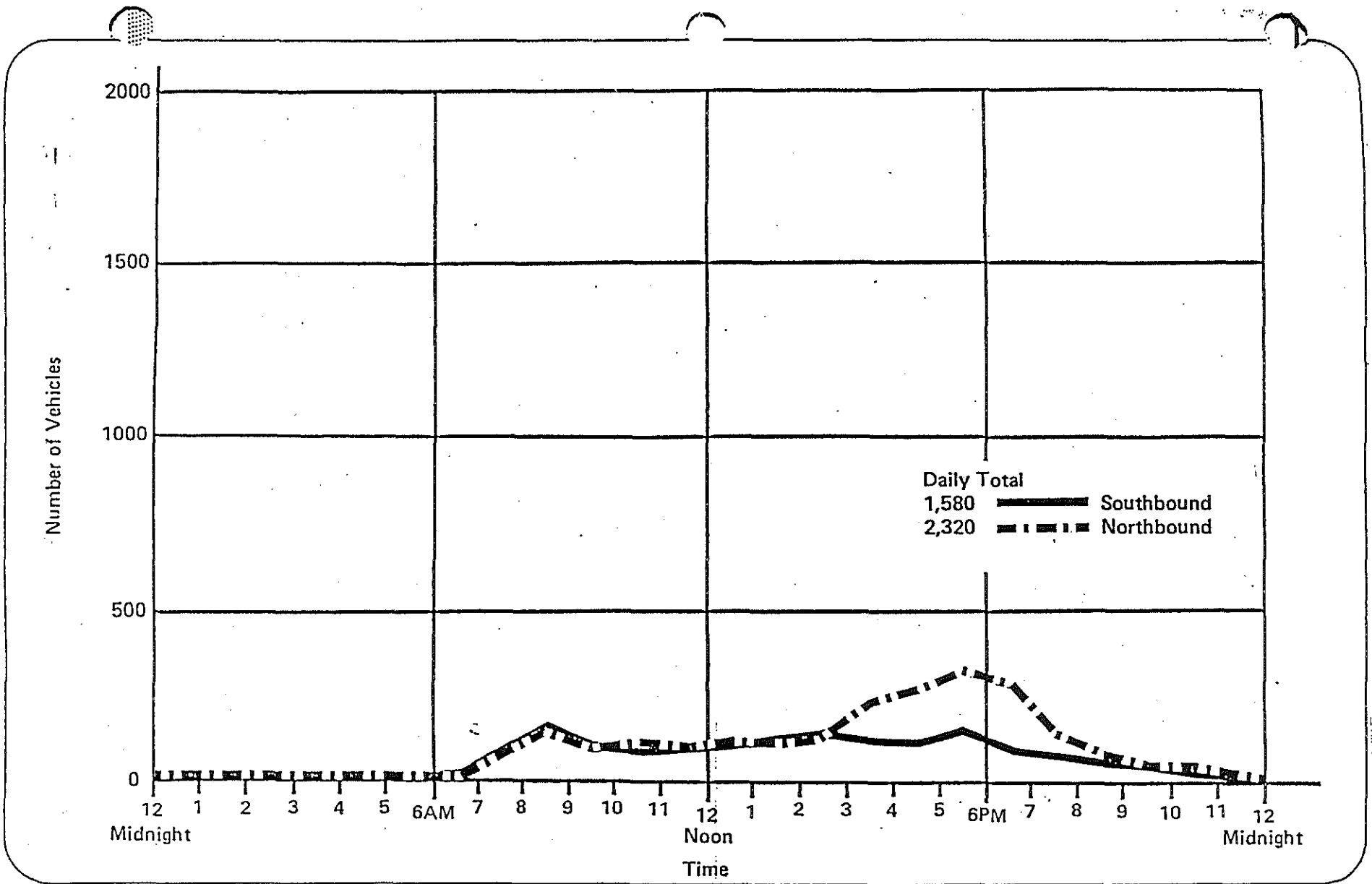
The intent of these guidelines is to provide attractive area and building lighting which contributes to the safe nighttime use of the area by residents, employees or visitors. The lighting should contribute to the overall design quality of the area and enhance the building architecture or landscape. Specific lighting fixtures should be chosen for their good design in daylight situations and for their contribution to the overall visual quality of the gateway area during the hours of darkness. Lighting fixtures which produce a glare for pedestrians or vehicle operators or exhibit motion or simulate motion shall not be avoided.

Future development plans shall:

- Include a lighting plan for parking areas which utilizes energy efficient and non-glare lamps.
- Provides lighting which eliminates observation of light sources.
- Place lighting where it can best aid in illuminating activity areas. The site should not be overly lit. Fixtures should be scaled in size to match the magnitude of areas to be lit.

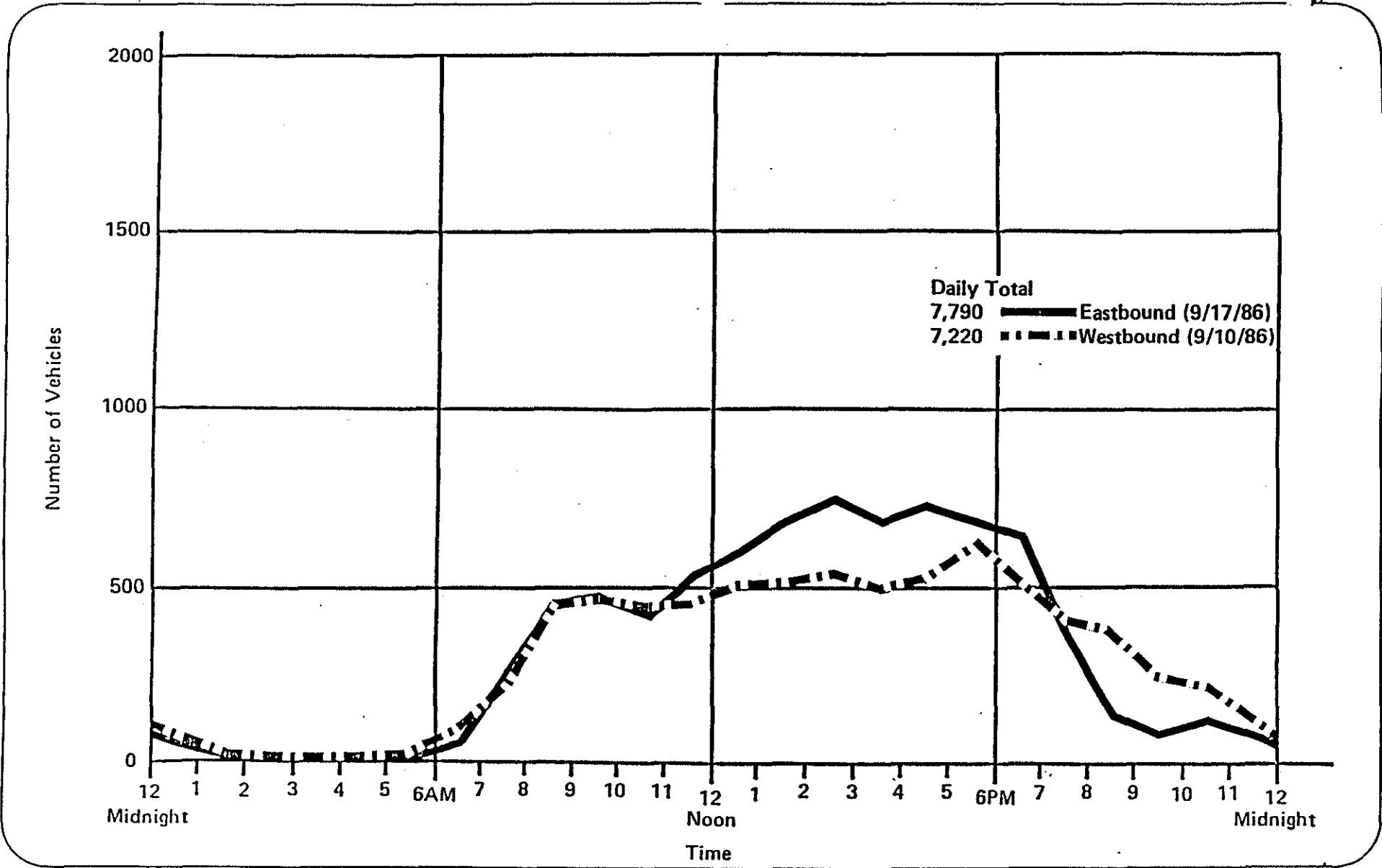
- Direct area lighting predominantly downward and place to prevent glare or excessive spray of light on neighboring sites.
- Provide accent illumination at key locations such as building entries and driveway entries.
- Include lighting fixtures to illuminate pedestrian walkways, plazas or other activity points.
- Provide lighting or highlighting of building facades that is sensitive, subtle and not excessive. Lighting must be adequate for health and safety.

APPENDIX E.
TRANSPORTATION

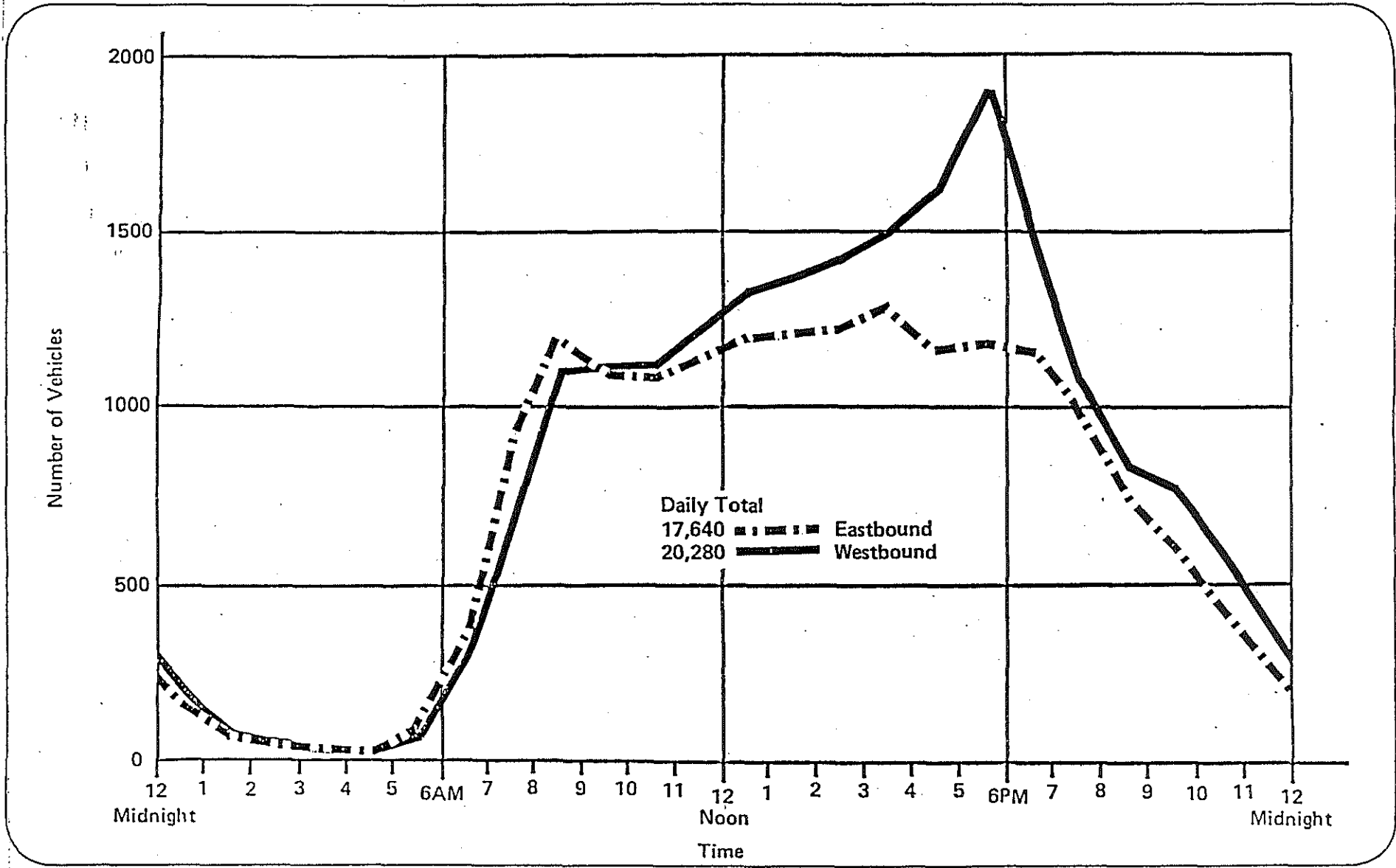


**HOURLY TRAFFIC VARIATIONS
CAMINO ALTO - BETWEEN VASCO & OVERHILL
(Weds. September 10, 1986)**

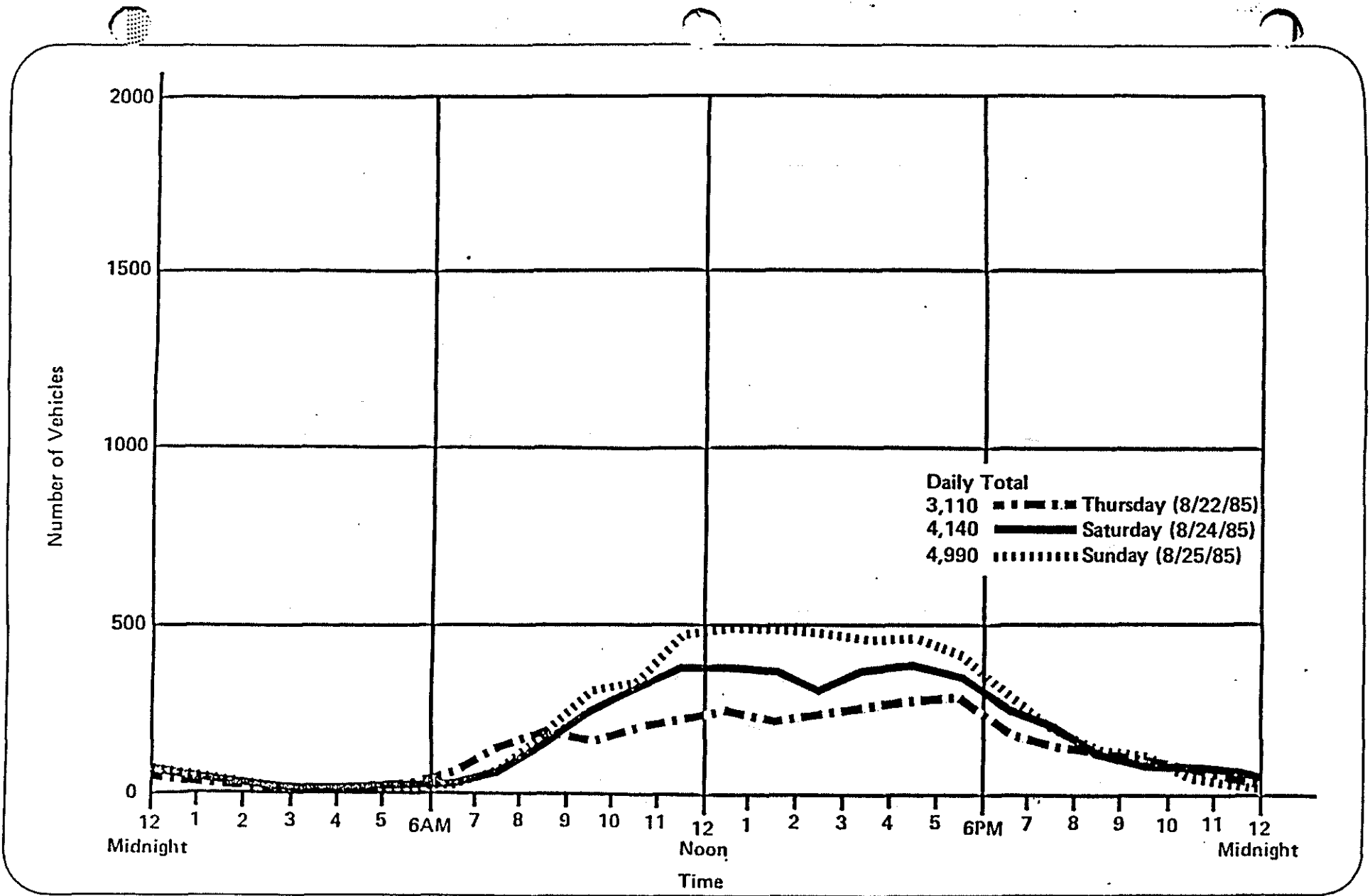
7-7



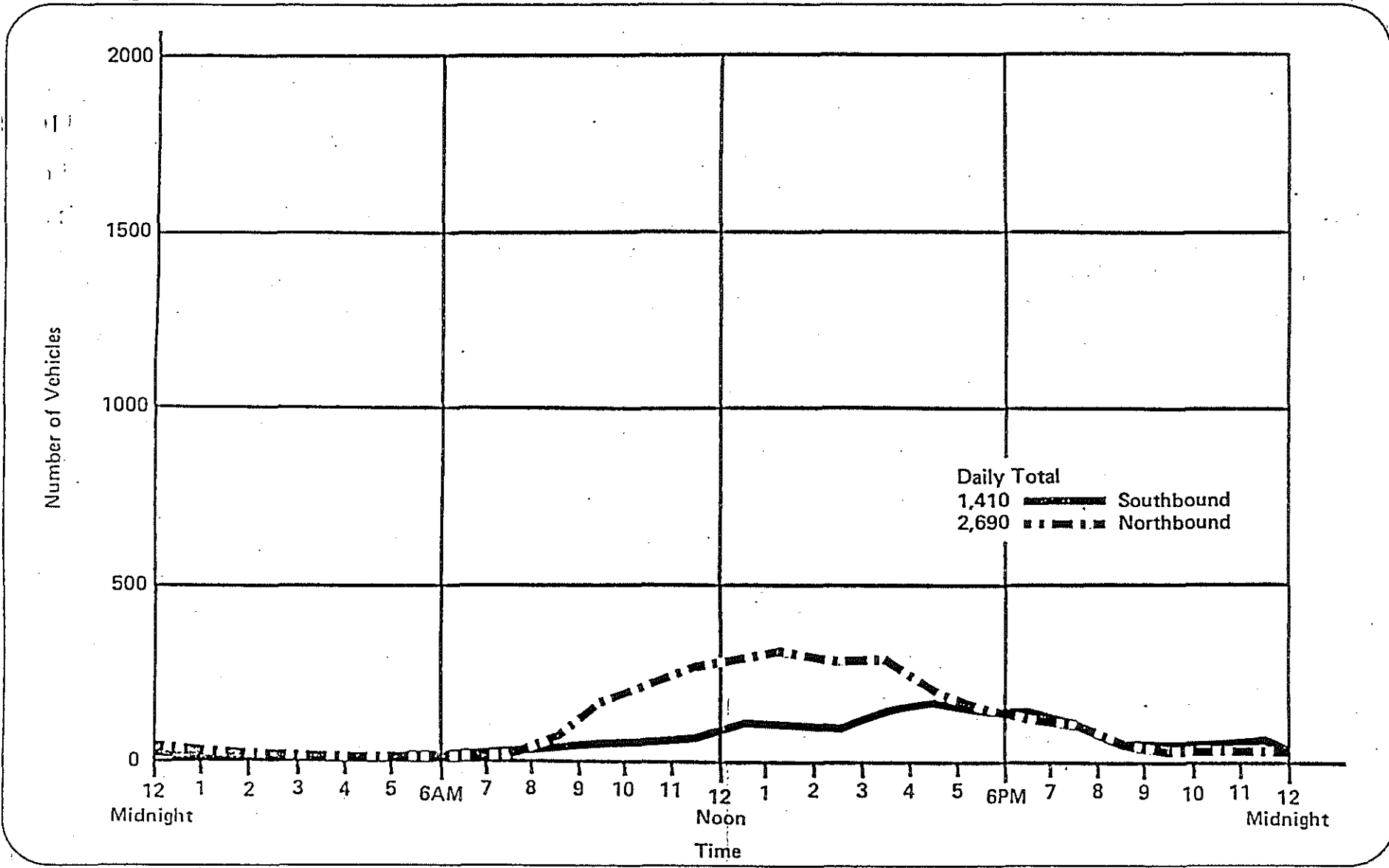
**HOURLY TRAFFIC VARIATIONS
BLITHEDALE - BETWEEN SYCAMORE & CARMELITA
(Weds. September 10, & September 17, 1986)**



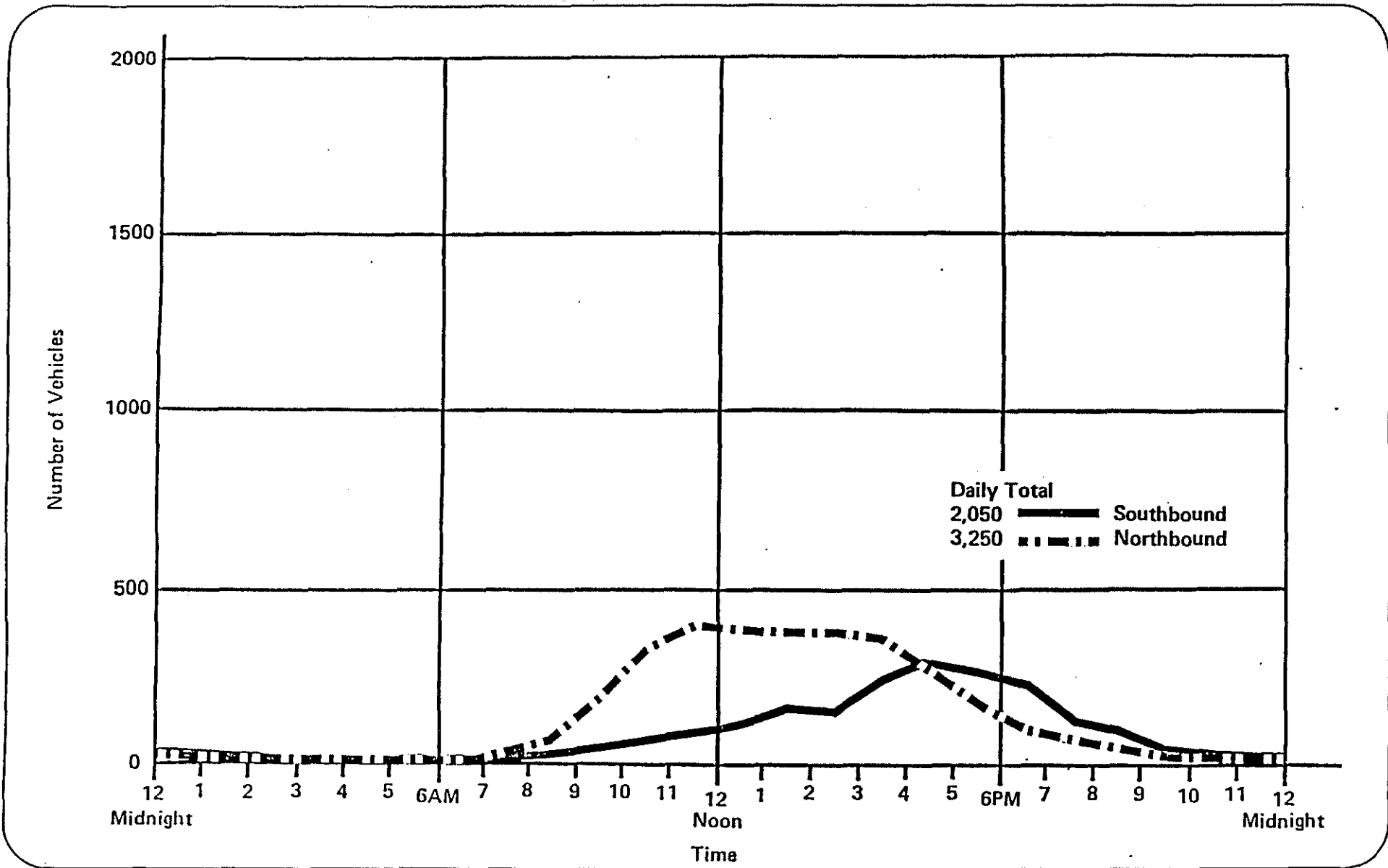
HOURLY TRAFFIC VARIATIONS
TIBURON BLVD - BETWEEN TOWER & HWY 101
(Weds. June 19, 1985)



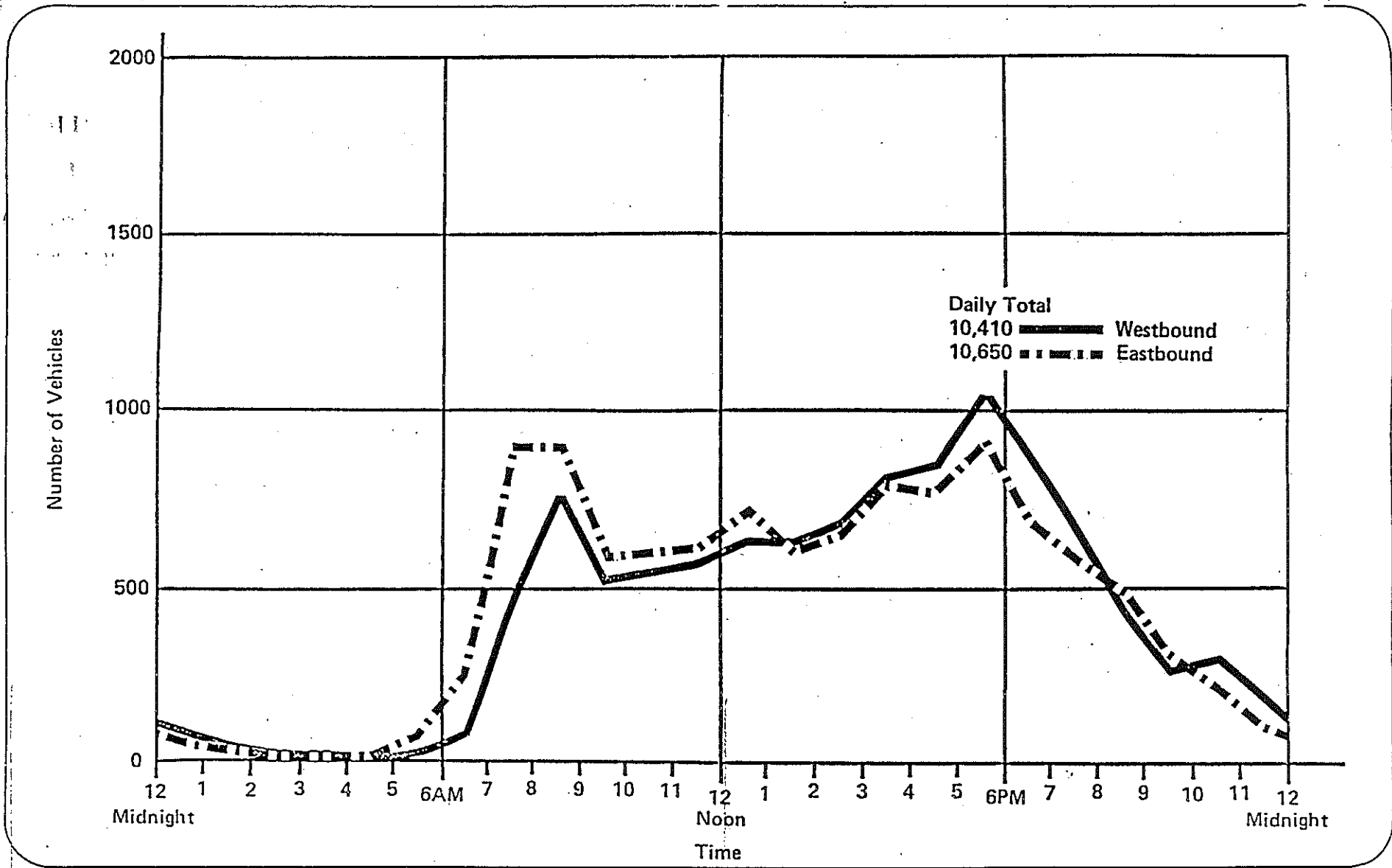
**TWO DIRECTIONAL HOURLY TRAFFIC VARIATIONS
 SEQUOIA VALLEY RD - NORTH OF PANORAMIC HWY
 (Thurs. August 22, Sat. August 24, & Sun. August 25, 1985)**



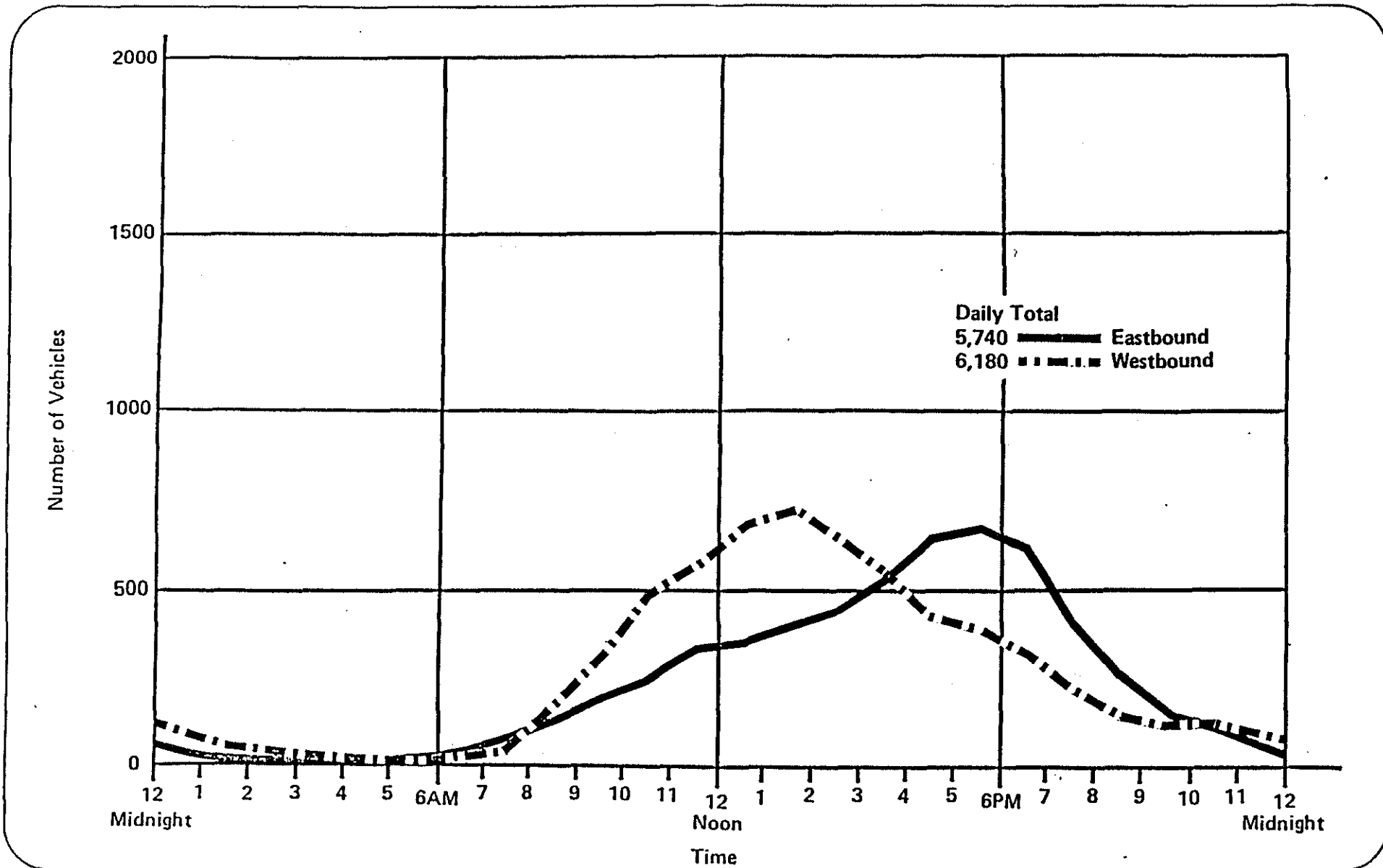
**HOURLY TRAFFIC VARIATIONS
PANORAMIC HWY - SOUTH OF MUIR WOODS RD
(Sat. August 17, 1985)**



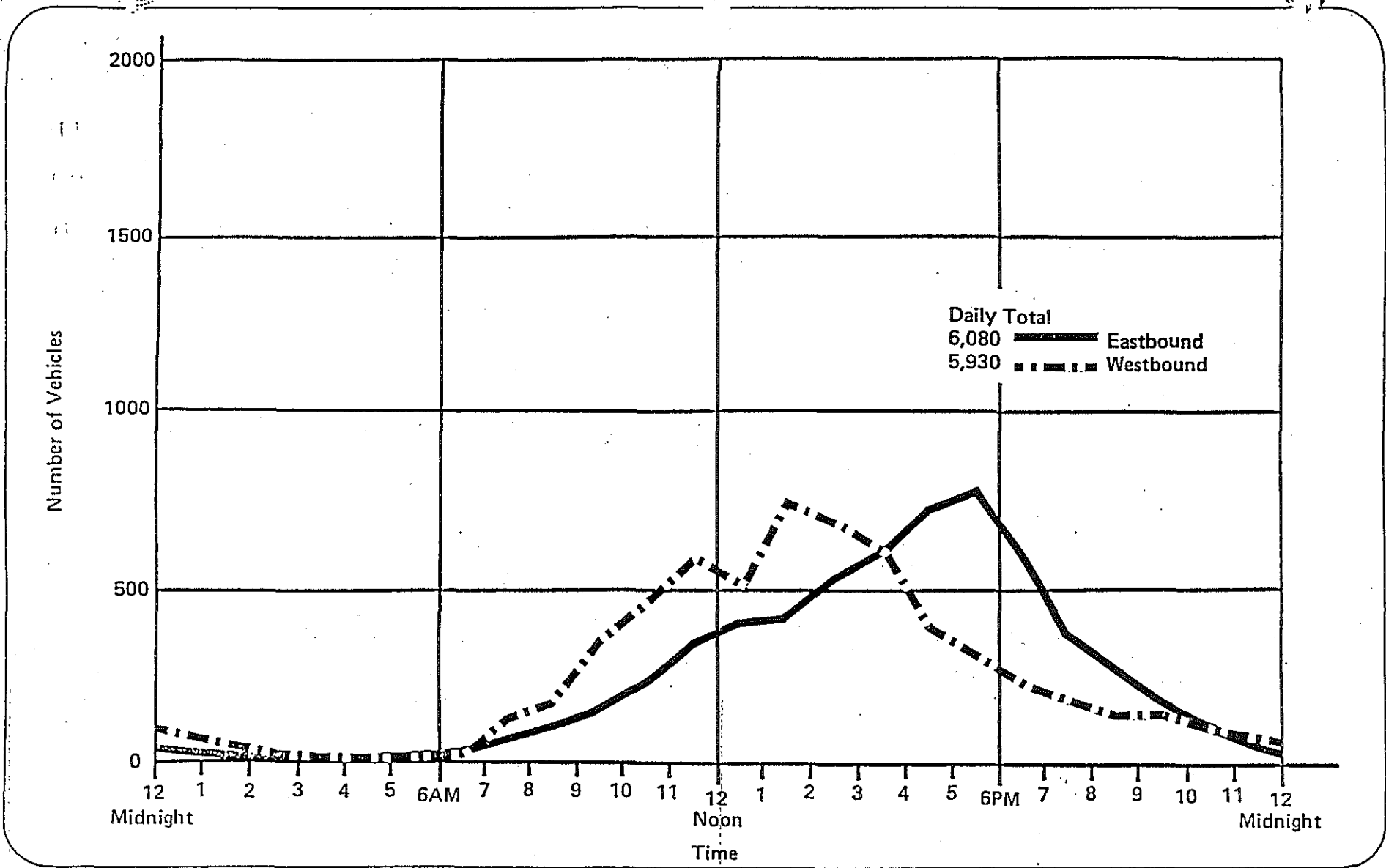
**HOURLY TRAFFIC VARIATIONS
PANORAMIC HWY - SOUTH OF MUIR WOODS RD
(Sun. August 18, 1985)**



**HOURLY TRAFFIC VARIATIONS
MILLER - BETWEEN CAMINO ALTO & ALMONTE
(Weds. September 10, 1986)**

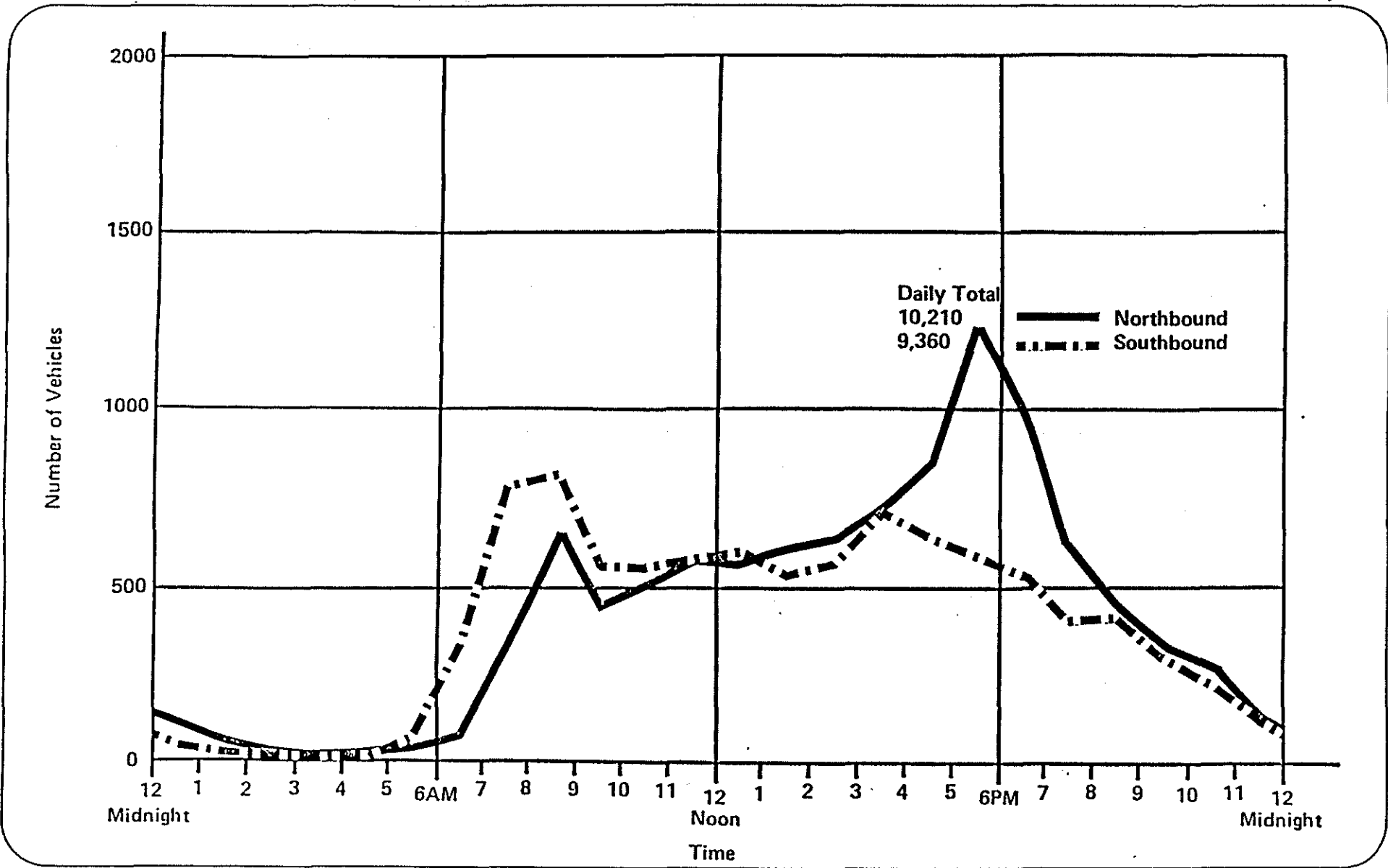


**HOURLY TRAFFIC VARIATIONS
SHORELINE HWY - SOUTH OF LORING
(Sat. August 2, 1986)**

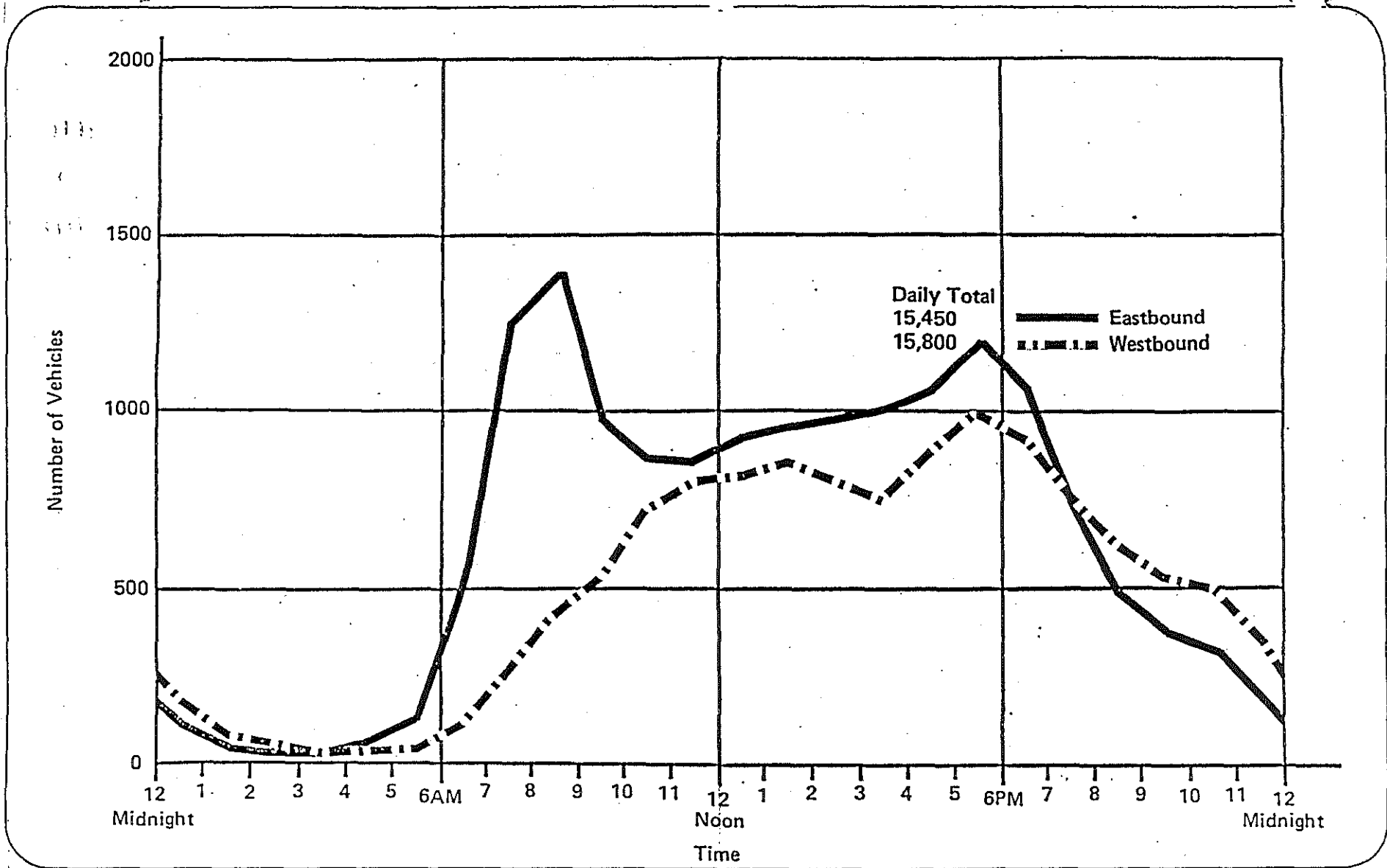


**HOURLY TRAFFIC VARIATIONS
SHORELINE HWY - SOUTH OF LORING
(Sun. August 10, 1986)**

BARTON-ASCIRIAN ASSOCIATES, INC.

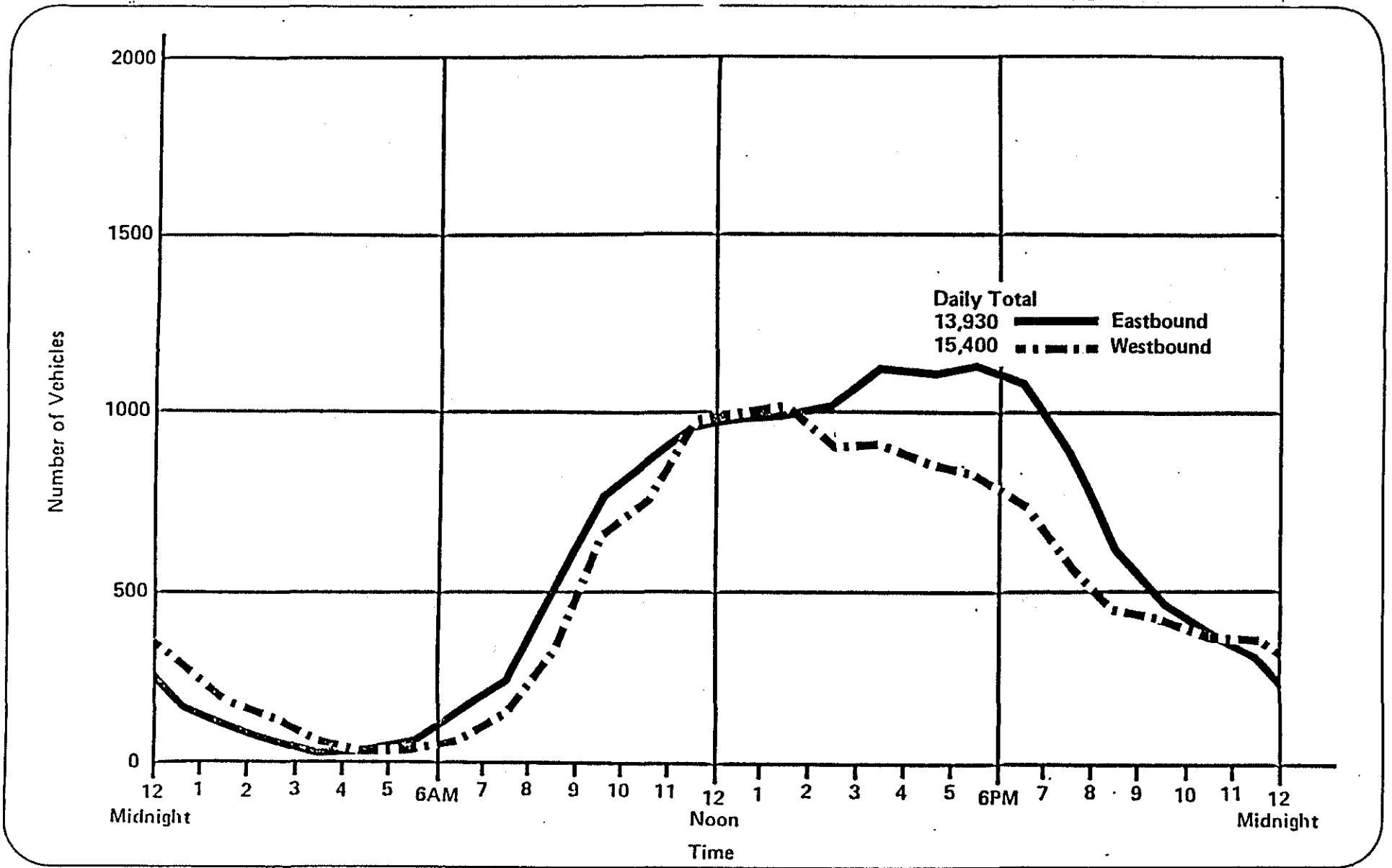


**HOURLY TRAFFIC VARIATIONS
ALMONTE BLVD - NORTH OF STATE ROUTE 1
(Weds. June 5, 1985)**

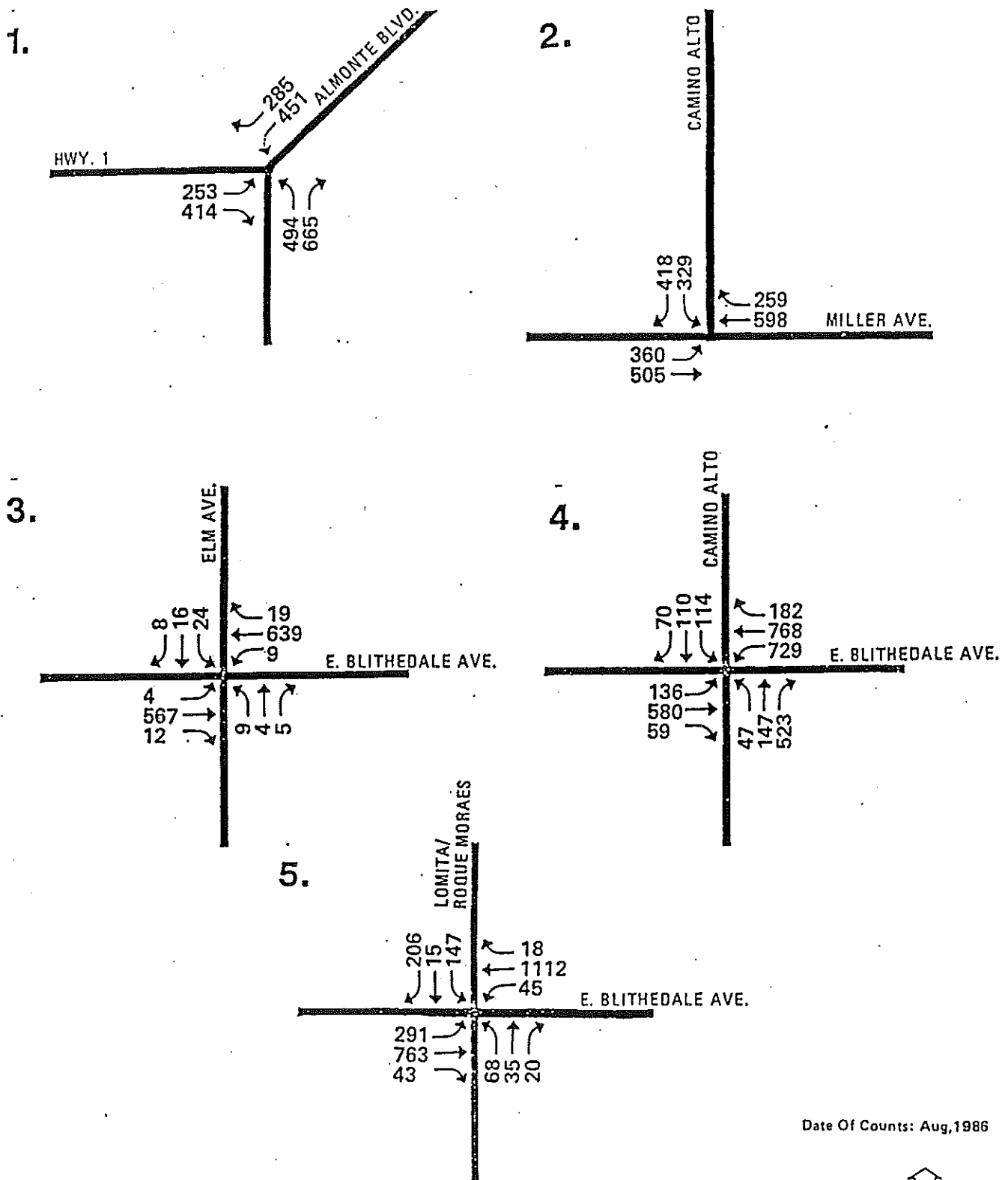


**HOURLY TRAFFIC VARIATIONS
SHORELINE HWY - EAST OF ALMONTE BLVD
(Thurs. July 31, 1986)**

BARTON-ASCHELIAN ASSOCIATES, INC.



**HOURLY TRAFFIC VARIATIONS
SHORELINE HWY - EAST OF ALMONTE BLVD
(Sat. August 9, 1986)**



Date Of Counts: Aug, 1986



Figure 16

EXISTING SIGNALIZED INTERSECTIONS VOLUMES IN THE PM PEAK HOUR

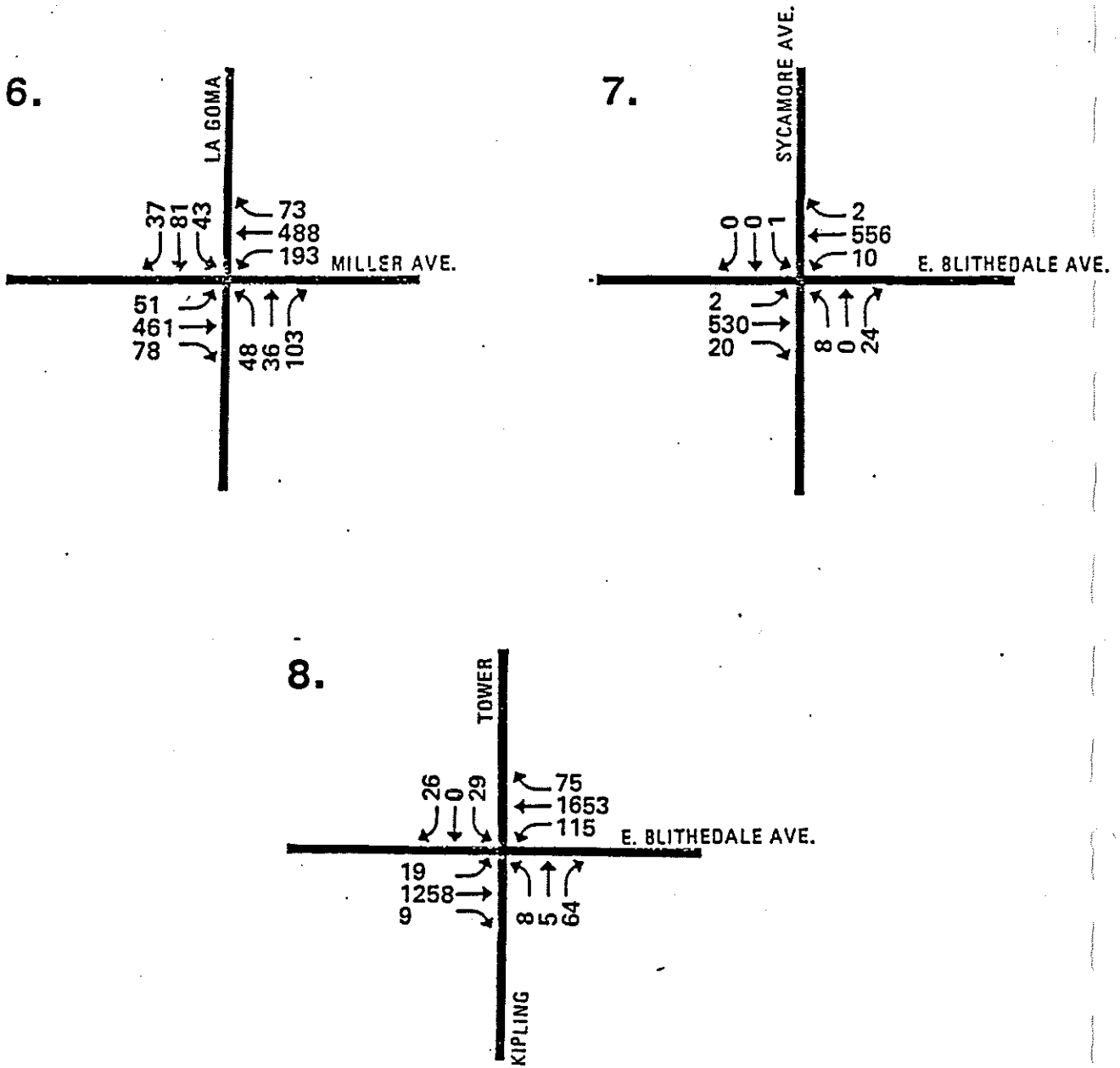
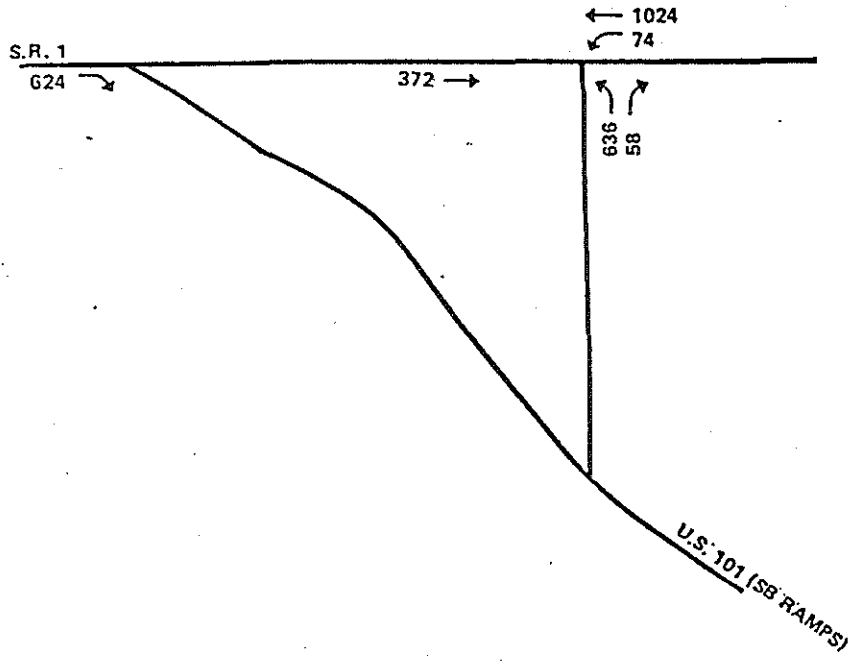


Figure 17

EXISTING UNSIGNALIZED INTERSECTION
VOLUMES IN THE PM PEAK HOUR



9.



10.

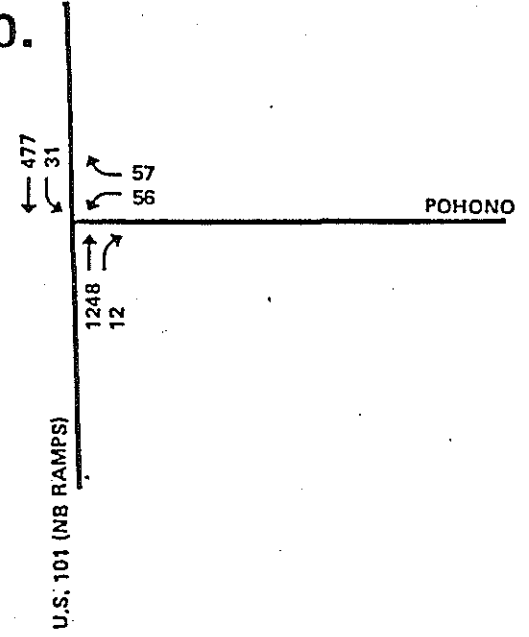
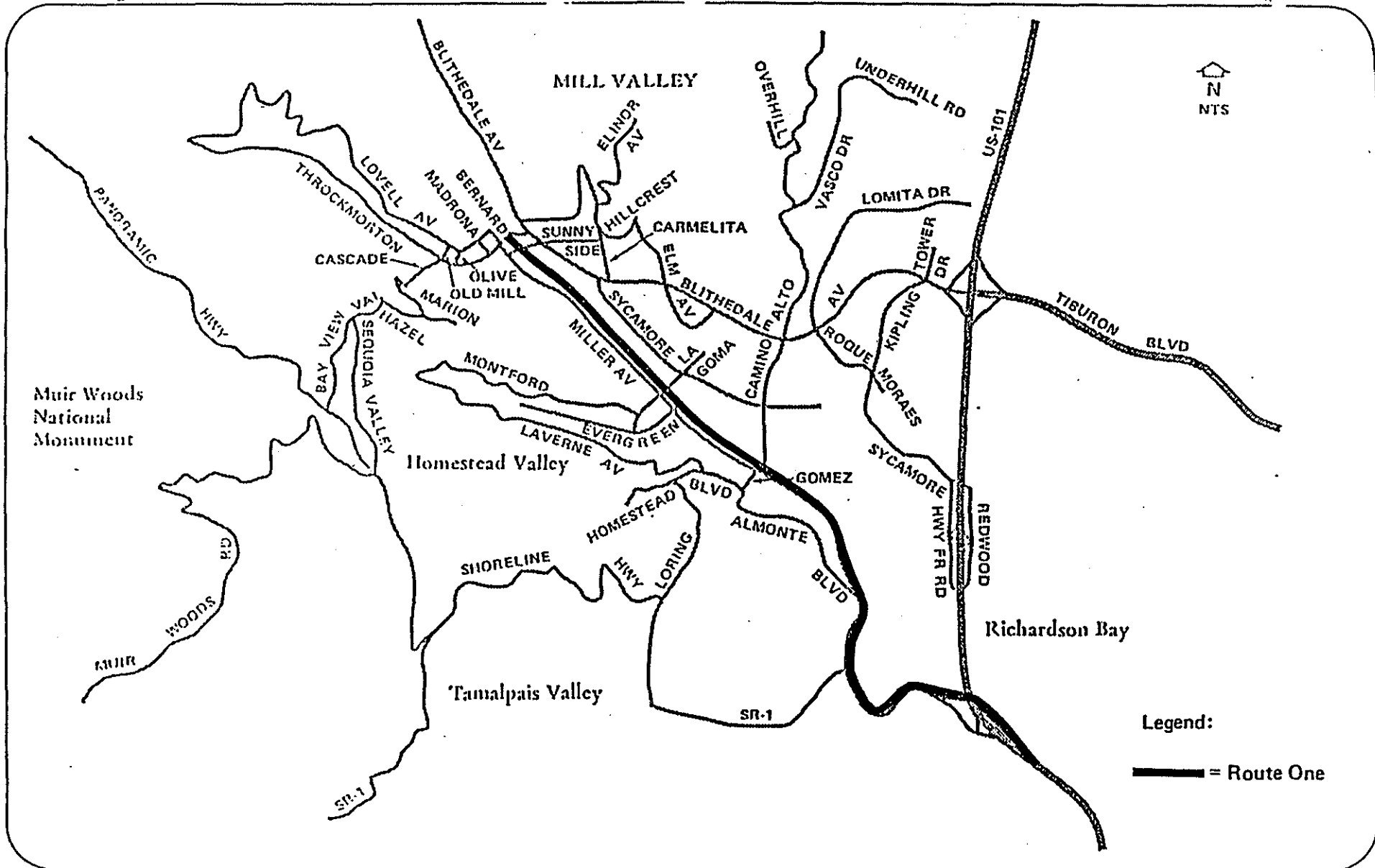
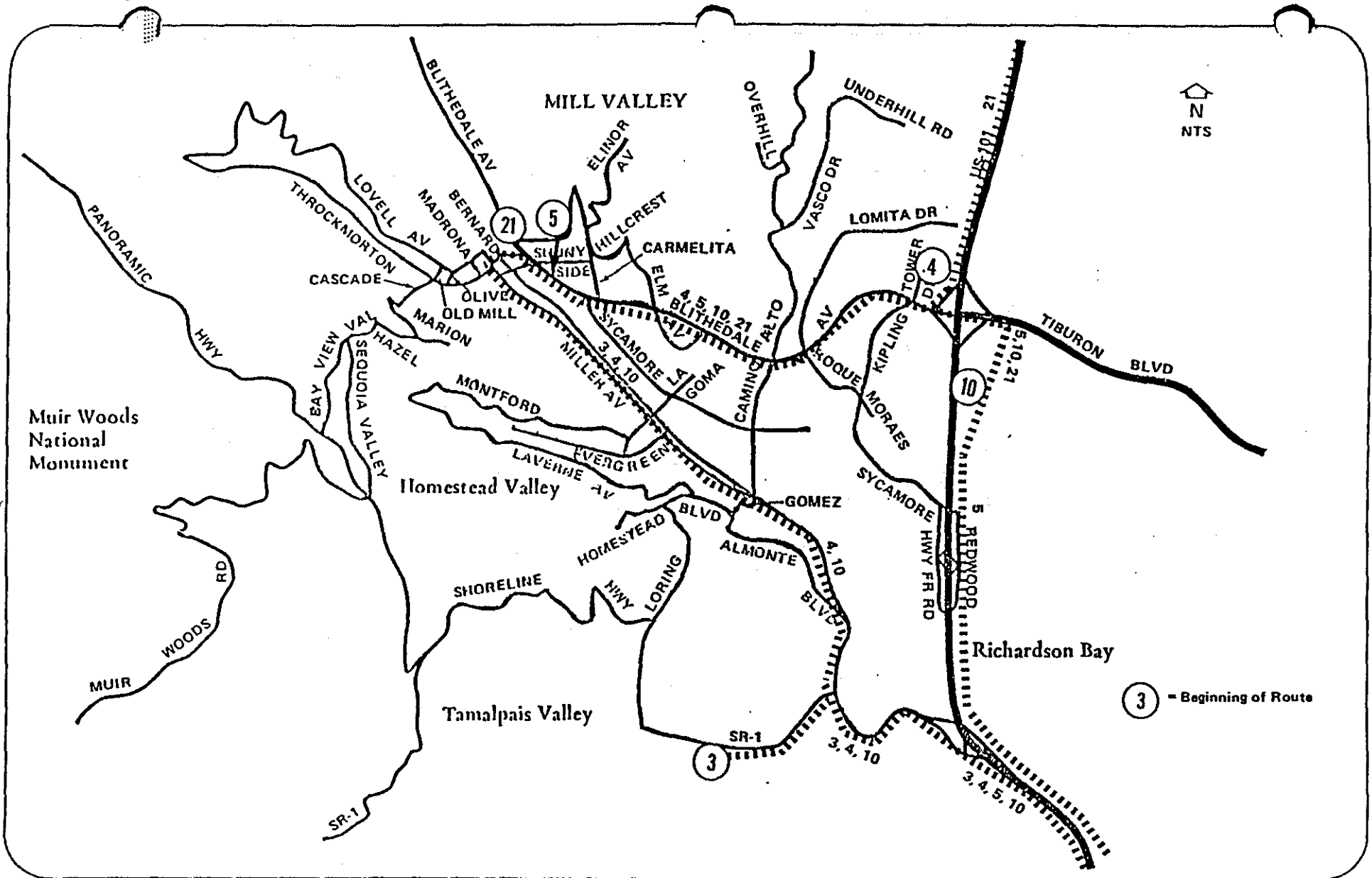


Figure 17

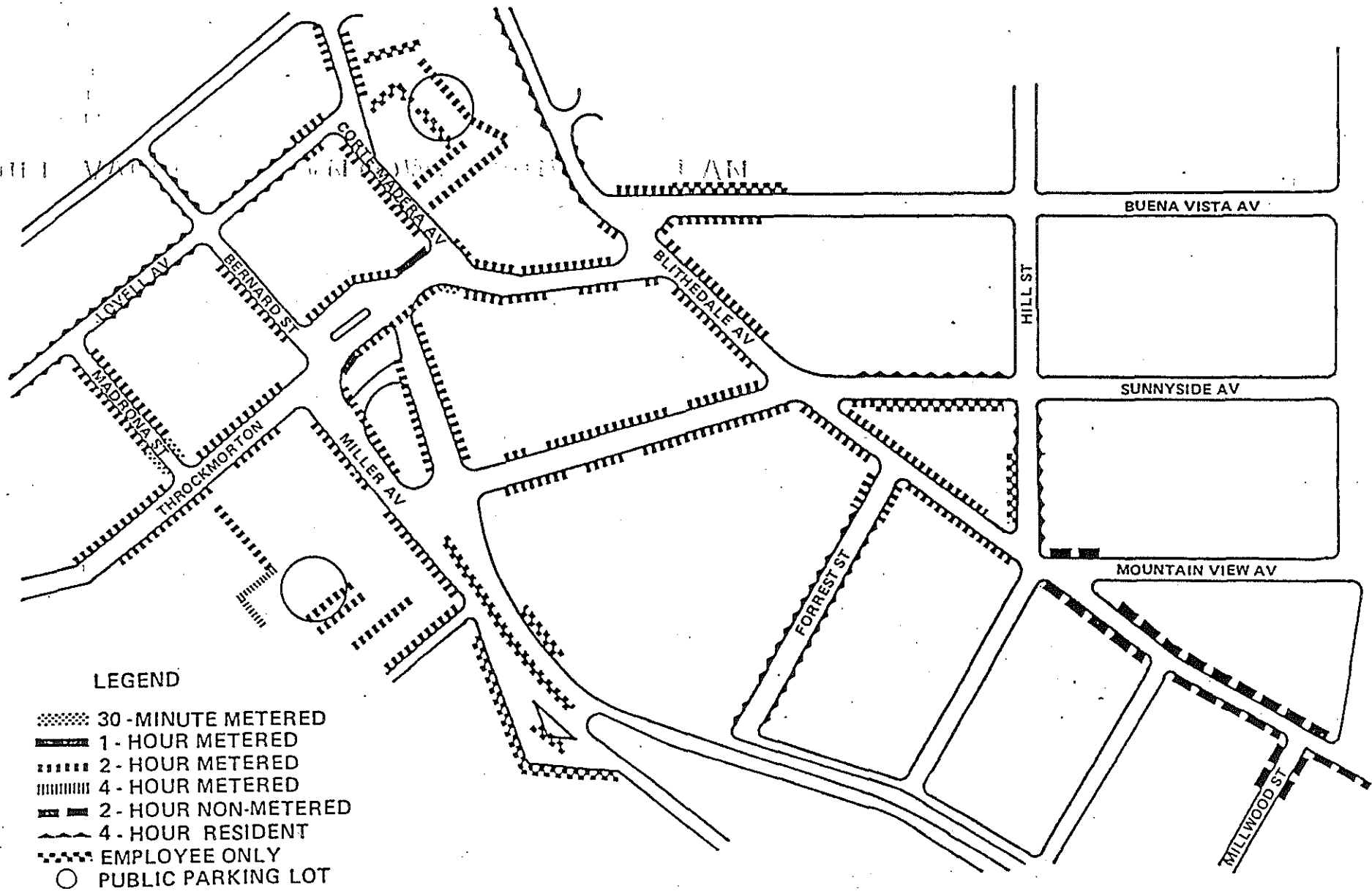
**EXISTING UNSIGNALIZED
INTERSECTION VOLUMES
IN THE PM PEAK HOUR (CONTINUED)**



TRAVEL TIME SURVEY ROUTES

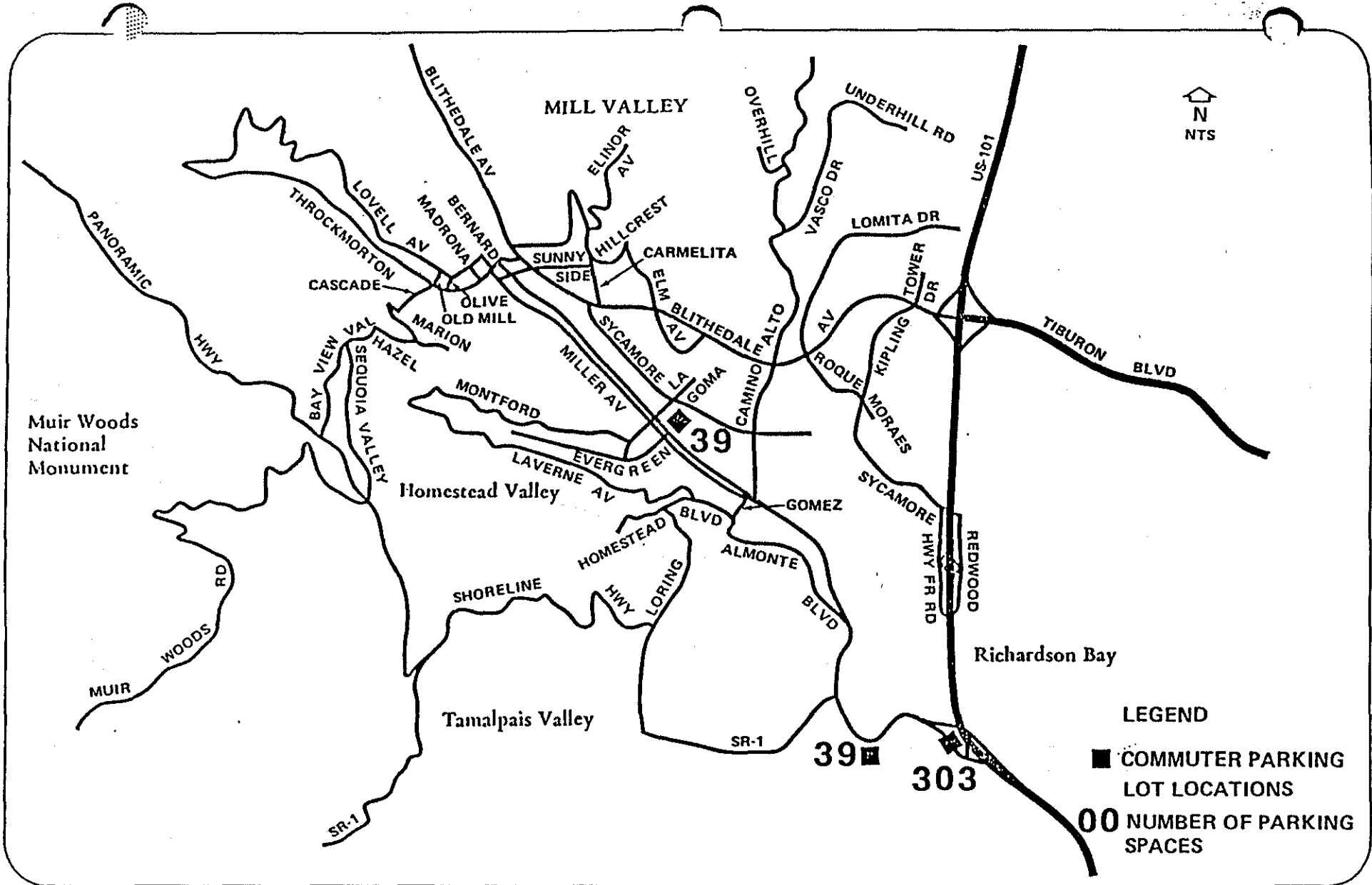


TRANSIT ROUTES PROVIDED BY GOLDEN GATE TRANSIT

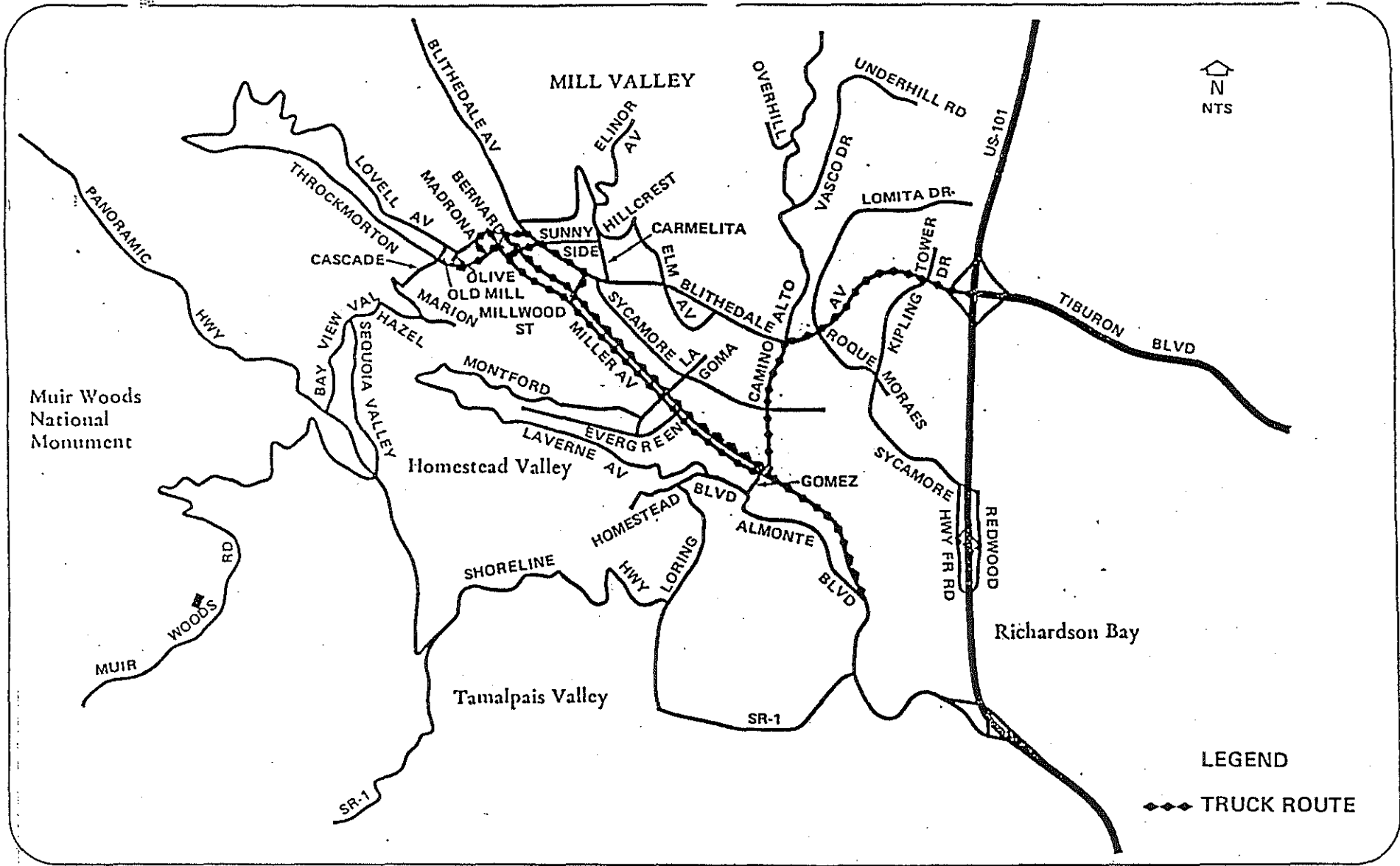


MILL VALLEY DOWNTOWN PARKING PLAN

Figure 23



COMMUTER PARKING LOTS



MILL VALLEY TRUCK ROUTES

BARTON-ASCHMAN ASSOCIATES, INC.

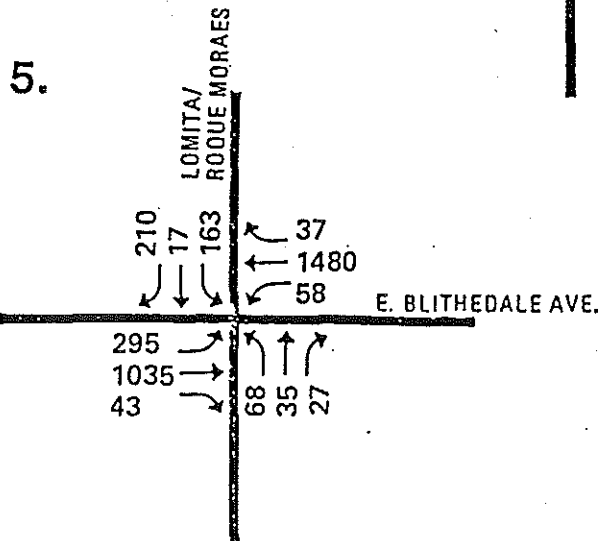
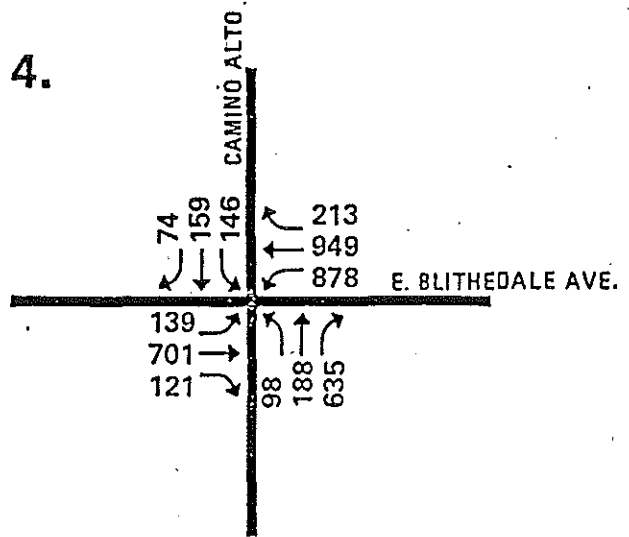
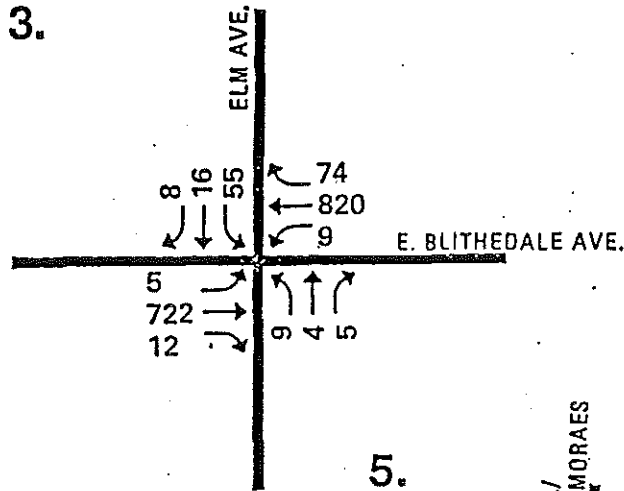
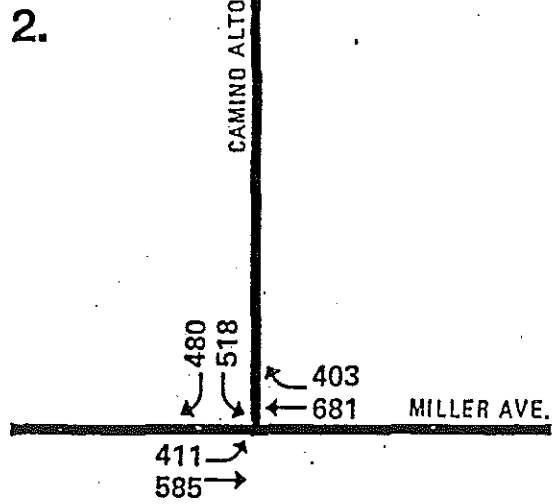
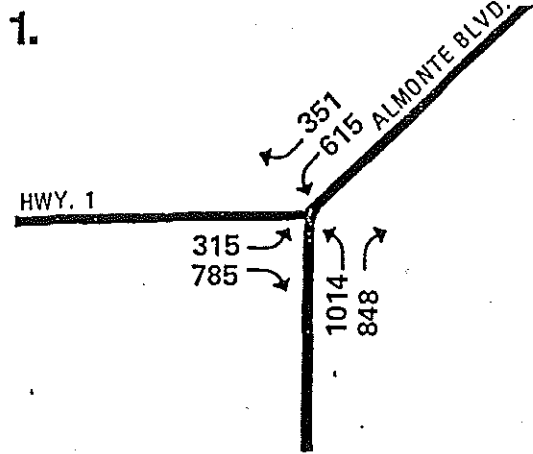
TABLE 9
YEAR 2005 JOURNEY-TO-WORK TRIP PATTERNS

From Mill Valley/Tamalpais Planning Area Residences:	
To North of Mill Valley	25%
To South of Mill Valley	52%
Internal	<u>23%</u>
	100%
To Mill Valley Jobs:	
From North of Mill Valley	56%
From South of Mill Valley	6%
Internal	<u>38%</u>
	100%

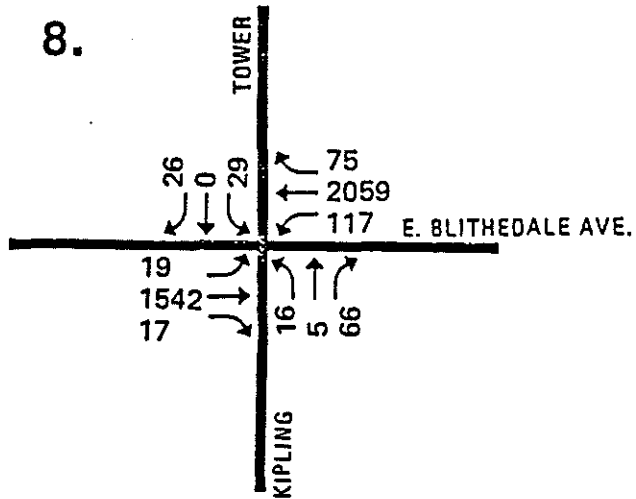
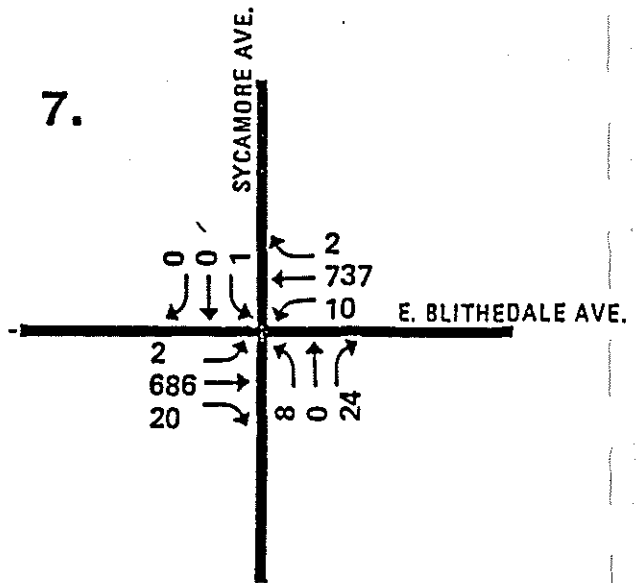
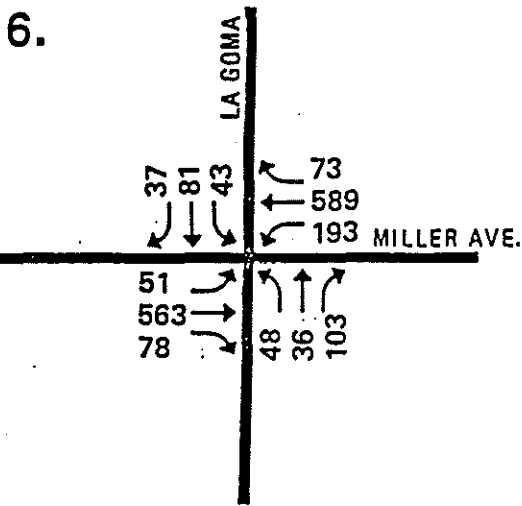
Sources: Metropolitan Transportation Commission, "Development of Year 2005 FRATAR Commuter and Total Worker Matrices," November 1985.

TABLE 10
TRIP DISTRIBUTION FORECASTS FOR PLANNING AREA BUILD-OUT

<u>Trip Pattern</u>	<u>To and From Residential Uses</u>	<u>To and From Commercial Uses</u>
Via U.S. 101, north of Mill Valley	44%	56%
Via U.S. 101, south of Mill Valley	52%	10%
Via Planning Area internal streets	<u>4%</u>	<u>34%</u>
	100%	100%



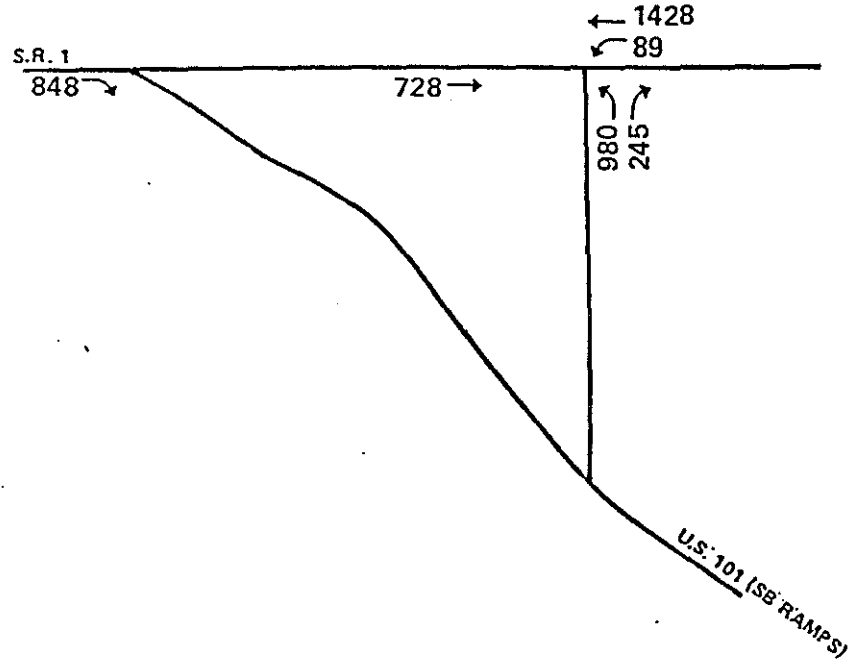
GENERAL PLAN BUILDOUT -
 SIGNALIZED INTERSECTION VOLUMES,
 PM PEAK HOUR



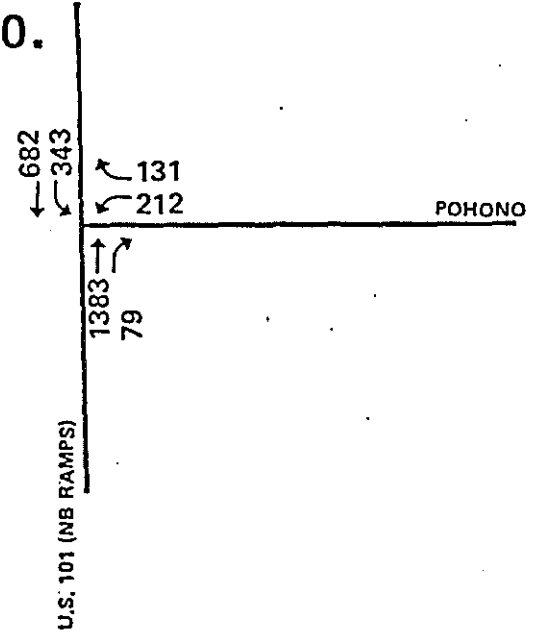
GENERAL PLAN BUILDOUT -
UNSIGNALIZED INTERSECTION VOLUMES,
PM PEAK HOUR



9.



10.



GENERAL PLAN BUILDOUT -
UNSIGNALIZED INTERSECTION VOLUMES
PM PEAK HOUR (CONTINUED)

COMMUTER TRAVEL TIME AND DELAY STUDY

In recent years concern has been expressed regarding the infiltration of commuter trips on local and collector streets within the City of Mill Valley. A travel time survey was designed and executed to determine the actual travel times for alternative commuter routes for Mill Valley residents who utilize U.S. 101 and travel to San Francisco and areas south of Mill Valley.

The travel time-and-delay survey was conducted for seven routes in and out (AM peak commute hours and PM peak commute hours) of Mill Valley. Of these seven routes, Route Number One, which travels U.S. 101 to Route 1 and up Miller Avenue, is the most convenient way due to time and distance involved. All runs were conducted between the Highway 101/Route 1 interchange and Mill Valley City Hall. The seven routes are as follows:

Route One: Exit Highway 101/Route 1 interchange and follow Route 1 to Tam Junction (Route 1 and Almonte Blvd. intersection). Follow Almonte Blvd. until it changes to Miller Avenue and follow Miller Avenue to Throckmorton, and turn right to downtown and City Hall. See Figure 17.

Route Two: Stay on Highway 101 until the E. Blithedale/Tiburon Blvd. exit and follow E. Blithedale to Throckmorton, and turn left to downtown and City Hall. See Figure 18.

Route Three: Exit Highway 101 at Route 1, and at Tam Junction take Almonte Blvd. to Miller Avenue. At the Miller Avenue/Camino Alto intersection, take Camino Alto north. At the E. Blithedale/Camino Alto intersection, take E. Blithedale west to Throckmorton and turn left on Throckmorton to downtown. See Figure 18.

Route Four: Exit Highway 101 at Route 1, and at Tam Junction take Almonte Blvd. to Miller Avenue. At the Miller Avenue/Camino Alto intersection go north on Camino Alto to Sycamore and then head west. Sycamore ends at E. Blithedale, so head west on E. Blithedale to Throckmorton and downtown. See Figure 19.

Route Five: Stay on Highway 101 to the E. Blithedale/Tiburon Blvd. exit and follow E. Blithedale. At the Camino Alto/E. Blithedale intersection, take Camino Alto south to Miller Avenue and head northwest on Miller Avenue. Take Miller Avenue to Throckmorton and downtown. See Figure 19.

Route Six: Highway 101 to Blithedale/Tiburon exit and follow E. Blithedale. At Camino Alto/E. Blithedale, go south on Camino Alto. At Sycamore Avenue, head west. Sycamore ends at E. Blithedale, so head west on E. Blithedale until Throckmorton and downtown. See Figure 20.

Route Seven: Take Highway 101 to the Seminary Drive Exit and head south to go underneath Highway 101. At Sycamore Avenue/Hamilton Drive, turn left. Take Hamilton Drive to Roque Moraes and turn left. At

the Roque Moraes/E. Blithedale intersection, head west on E. Blithedale until Throckmorton and downtown. See Figure 20.

Table 1 summarizes the data found on each of the seven routes. Some routes were not conducted in the morning because they are lengthy and are not convenient routes for the majority of Mill Valley residents.

AM Outbound: There is very little traffic, so the only delays occurred at the stop lights. Route One (the Miller Avenue route) seemed to have a slight advantage (approximately 10 seconds) because it had only two lights, compared with the three lights on Route Two (the E. Blithedale route) and Route Three (the E. Blithedale/Camino Alto/Miller route).

PM Inbound: Significant traffic in the afternoon adds 30 to 90 seconds to the commute time. Route One (the Miller route) seems to be more desirable than Route Two (the E. Blithedale route) because:

1. It is shorter (3.2 miles vs. 3.9 miles);
2. It has fewer lights (two vs. five); and
3. It has more lanes in the City (four vs. two).

The number of lanes is significant because of delays caused by buses, cars turning left, and cars parking on E. Blithedale.

Route Three (the Miller/Camino Alto/E. Blithedale route) is comparable to the others, but in the long run it will not prove to be as efficient as Route One because: it is longer, has one more light, and must contend with the E. Blithedale congestion.

Route Four (the Almonte/Camino Alto/Sycamore/E. Blithedale route) has low mileage, but driving on the residential street of Sycamore is slow because of curbside parking, it is residential, and the street is in poor condition. This route takes longer, and with no control (stop sign or traffic signal) at the Sycamore/E. Blithedale intersection, the delay caused by waiting for a break in traffic can be lengthy.

Routes Five, Six, and Seven are time-consuming and have the longest delays of the seven routes due to traffic signals or lengthy drives, or due to waiting at a stop sign for traffic to clear.

Route One (the Miller route) is the optimum route; it is the shortest and it has the lowest inbound and outbound travel time in the commuting hours.

COST ESTIMATE

IMPROVEMENT PROJECT: Shoreline Highway (State Route 1)
 Pohono Street to West Side of Howard Johnson
 March 14, 1991 Art Brook
 Revise March 23, 1991

MATERIAL	UNIT	UNIT COST	QUANTITY	COST
PAVING:				
Paving (see footnote)	Sq. Ft.	2.65	40625	\$108,062.50
Sidewalk	Sq. Ft.	4.00	11440	\$45,760.00
Curb & Gutter	Lin. Ft.	10.00	4060	\$40,600.00
Overlay & Concrete Paving	Sq. Ft.	1.25	46000	\$57,500.00
DRAINAGE:				
Main - 30 Inch Reinforced Concrete Pipe	Lin. Ft.	60.00	840	\$50,400.00
Storm Drainage Manholes	Each	1500.00	4	\$6,000.00
Lateral - 12 Inch Reinforced Concrete Pipe	Lin. Ft.	40.00	350	\$14,000.00
Catchbasins	Each	1000.00	9	\$9,000.00
UTILITIES:				
Sanitary Sewer Main	Lin. Ft.	70.00		\$0.00
Sanitary Sewer Manhole	Each	2000.00		\$0.00
Sanitary Sewer Lateral	Lin. Ft.	40.00		\$0.00
TRAFFIC CONTROL:				
Signing and Striping	Lump Sum		5000	\$5,000.00
Signals	Lump Sum		125000	\$125,000.00
GRADING:				
Cut	Cubic Yard	5.00		\$0.00
Fill	Cubic Yard	12.00	50	\$600.00
Retaining Walls to 8 feet	Cubic Yard	500.00	28	\$14,000.00
GRUBBING:				
Grubbing	Lump Sum		5000	\$5,000.00
RIGHT OF WAY:				
Right of Way	Sq. Ft.	25.00		\$0.00
GUARDRAIL:				
Ends	Each	500.00	2	\$1,000.00
Sections	Lin Foot	27.50	70	\$1,925.00
SPECIAL CONSIDERATION:				
SUBTOTAL				\$406,347.50
CONTINGENCIES:				\$97,269.50
DESIGN AND ENGINEERING:				\$121,586.88
TOTAL				\$705,203.88

COST ESTIMATE

IMPROVEMENT PROJECT: Shoreline Highway (State Route 1)
 Coyote Creek Bridge to Almonte/Flamingo
 5 Lane Section
 March 14, 1991 Art Brook

MATERIAL	UNIT	UNIT COST	QUANTITY	COST
PAVING:				
Paving (see footnote)	Sq. Ft.	2.66	76800	\$209,608.00
Sidewalk	Sq. Ft.	4.00	13280	\$53,120.00
Curb & Gutter	Lin. Ft.	10.00	3560	\$35,600.00
Overlay & Conform Paving	Sq. Ft.	1.25	62400	\$78,000.00
DRAINAGE:				
Main - 30 Inch Reinforced Concrete Pipe	Lin. Ft.	60.00	660	\$39,600.00
Storm Drainage Manholes	Each	1500.00	3	\$4,500.00
Lateral - 12 Inch Reinforced Concrete Pipe	Lin. Ft.	40.00	523	\$20,920.00
Catchbasins	Each	1000.00	10	\$10,000.00
UTILITIES:				
Sanitary Sewer Main	Lin. Ft.	70.00		\$0.00
Sanitary Sewer Manhole	Each	2000.00		\$0.00
Sanitary Sewer Lateral	Lin. Ft.	40.00		\$0.00
TRAFFIC CONTROL:				
Signing and Striping	Lump Sum		3000	\$3,000.00
Signals	Lump Sum		193000	\$193,000.00
GRADING:				
Cut	Cubic Yard	5.00		\$0.00
Fill	Cubic Yard	12.00		\$0.00
Retaining Walls to 8 feet	Cubic Yard	500.00		\$0.00
GRUBBING:				
Grubbing	Lump Sum			\$0.00
RIGHT OF WAY:				
Right of Way	Sq. Ft.	25.00	40000	\$1,000,000.00
GUARDRAIL:				
Ends	Each	500.00		\$0.00
Sections	Lin Foot	27.50		\$0.00
SPECIAL CONSIDERATION:				
SUBTOTAL				\$1,647,348.00
CONTINGENCIES:				0.2
				\$329,469.60
DESIGN AND ENGINEERING:				
Design and Engineering				0.25
				\$411,837.00
TOTAL				\$2,388,654.60

COST ESTIMATE

IMPROVEMENT PROJECT: Shoreline Highway (State Route 1)
 West Side of Howard Johnson to Coyote Creek Bridge
 Three Lane Section (Two Eastbound and One Westbound Lane)
 March 14, 1991 Art Brook

MATERIAL	UNIT	UNIT COST	QUANTITY	COST
PAVING:				
Paving (see footnote)	Sq. Ft.	2.66	23250	\$61,845.00
Sidewalk	Sq. Ft.	4.00		\$0.00
Curb & Gutter	Lin. Ft.	10.00		\$0.00
Overlay & Conform Paving	Sq. Ft.	1.25	30250	\$37,812.50
DRAINAGE:				
Main - 30 Inch Reinforced Concrete Pipe	Lin. Ft.	60.00	700	\$42,000.00
Storm Drainage Manholes	Each	1500.00	2	\$3,000.00
Lateral - 12 Inch Reinforced Concrete Pipe	Lin. Ft.	40.00	100	\$4,000.00
Catchbasins	Each	1000.00	4	\$4,000.00
UTILITIES:				
Sanitary Sewer Main	Lin. Ft.	70.00		\$0.00
Sanitary Sewer Manhole	Each	2000.00		\$0.00
Sanitary Sewer Lateral	Lin. Ft.	40.00		\$0.00
TRAFFIC CONTROL:				
Signing and Striping	Lump Sum		1000	\$1,000.00
Signals	Lump Sum			\$0.00
GRADING:				
Cut	Cubic Yard	5.00		\$0.00
Fill	Cubic Yard	12.00	1388	\$16,656.00
Retaining Walls to 8 feet	Cubic Yard	500.00	167	\$83,500.00
GRUBBING:				
Grubbing	Lump Sum			\$0.00
RIGHT OF WAY:				
Right of Way	Sq. Ft.	25.00		\$0.00
GUARDRAIL:				
Ends	Each	500.00	2	\$1,000.00
Sections	Lin Foot	27.50	800	\$22,000.00
SPECIAL CONSIDERATION:				
SUBTOTAL				\$276,813.50
CONTINGENCIES: 0.2				\$55,362.70
DESIGN AND ENGINEERING:				
Design and Engineering	0.25			\$69,203.38
TOTAL				\$401,379.58

1) AC paving at \$35 @ ton with a density of 145#/ cu ft and an assumed depth of 3 inches,
with base at \$30 @ton with a density of 135#/ cu ft and an assumed depth of 12 inches.

0.631375 AC 2.025 BASE 2.659375 Total

APPENDIX F

**ADMINISTRATIVE POLICIES
TAMALPAIS DESIGN REVIEW BOARD**

TAM DESIGN REVIEW BOARD

305 Bell Lane, Mill Valley, CA 94941

ADMINISTRATION POLICIES

October 12, 1989

- I. MEETING SCHEDULE: Meetings to consider design review applications are held on the second and fourth Thursdays of each month, as follows:

TIME: 7:00 p.m.

LOCATION: Conference Room
Community Services District Building
305 Bell Lane, Mill Valley

No meetings will be held on major holidays (e.g., Thanksgiving, Christmas, Fourth of July, etc.). If necessary, the Board may designate another meeting date to take the place of a cancelled meeting.

The Board may schedule additional meetings to handle administrative business, special projects, or to accommodate a temporary high volume of design review applications.

Regular Board meetings shall adjourn no later than 10:30 p.m., unless all Board members present agree to extend the meeting to a later time. In the event it is impossible to consider all agenda items before adjourning, the Board shall promptly designate a time when those items can be considered.

The Board shall make every effort to advise Applicants in advance of the possibility that their applications may not be considered as scheduled and that an alternate time will be arranged.

II. BOARD MEMBERSHIP: The Board shall consist of eight (8) Board members, as follows:

- (2) Architects or Building Designers
- (1) Real Estate Agent or Broker
- (1) Contractor or Engineer
- (1) Landscape Architect or Designer
- (2) At-Large Citizen Members
- (1) Merchant or Business Owner, whose place of business is located in the Planning Area

All Board members must reside in the Planning Area. However, the Board may decide to accept a non-resident for Board membership if the candidate demonstrates special expertise or local experience relevant to the Planning Area. No more than one (1) non-resident member may serve on the Board at one time.

All Board members are appointed by the Marin County Board of Supervisors. In selecting new candidates for membership, every effort should be made to seek technically-qualified individuals representing the different neighborhoods within the Planning Area (e.g., Homestead Valley, Almonte, Tam Valley, etc.).

The Board may request that the County of Marin increase or decrease the number of Board members as its needs warrant.

III. TERM LENGTH: All terms are for three (3) years, staggered.

A Board member may serve no more than two (2) consecutive terms. A Board member may not apply for appointment to a third term until at least one year following the expiration of his/her second term. Board members seeking a second term must be re-appointed by the Board of Supervisors.

TAM DESIGN REVIEW BOARD ADMINISTRATION POLICIES

October 12, 1989

Page 3

IV. ELECTION OF OFFICERS: The Board shall elect officers annually at the first meeting of the year. These offices shall be:

Chairperson: Responsible for conducting meetings, preparing agendas, taking minutes, writing Board correspondence, distributing minutes to the Planning Department

Vice-Chair: Responsible for assisting Chairperson as necessary, conducting meetings in absence of Chairperson

Secretary: Responsible for receiving design review applications from the County and confirming agendas with Applicants, conducting meetings in absence of Chairperson and Vice-Chair

The Board may designate additional offices it deems necessary to distribute the tasks of the Board.

V. MEETING ATTENDANCE: Board members will be allowed three (3) unexcused consecutive absences each year. Board members absent for more than three consecutive meetings, or a total of six (6) meetings per year, may be asked to resign their positions.

In order to be excused from a meeting, Board members should advise the Chairperson well in advance of an anticipated absence.

VI. LEAVE OF ABSENCE: A Board member may request a leave of absence from his/her Board duties for personal or business reasons, provided that only one (1) leave is taken during a three-year term of office and is approved by the Board.

October 12, 1989

Page 4

VII. VOTING: In the event of a tie vote, the presiding officer's vote shall break the tie.

VIII. CONFLICT OF INTEREST AND ABSTENTIONS: In the event a conflict of interest arises between a Board member and an Application, or an Applicant, under consideration by the Board, the Board member must abstain from voting on the project.

A conflict of interest may arise in the following circumstances:

The Board member owns, or has a legal interest in, the property or project being considered.

The Board member presently acts, or has acted in the past, as a representative of the Owner or Applicant of the project being considered (e.g., designer, architect, owner's spokesperson, engineer, attorney, etc.).

The Board member owns, or has a legal interest in, adjacent property that will benefit economically by completion of the project.

IX. PROFESSIONALISM: All Board members shall conduct themselves in a business-like manner at Board meetings and when acting on behalf of the Board. Each should keep in mind that the effectiveness of the entire Board is dependant upon the high level of professionalism demonstrated by each Board member.

All correspondence and other communications shall be reviewed and authorized by the Board prior to initiation. This shall not prohibit a Board member from speaking publicly about a project as an individual resident of the community.

All Board members shall make every effort to visit the sites of proposed projects prior to the Board meeting when they

TAM DESIGN REVIEW BOARD ADMINISTRATION POLICIES

October 12, 1989

Page 5

will be considered. The Board may designate certain Board members to visit individual projects and report back to the Board.

Under no circumstances shall a Board member communicate directly with community members or neighbors of a project under consideration with the intent of influencing the community's or neighbors' opinions about that project. To do so places the Board member in a position of advocacy above and beyond that which is appropriate in carrying out the Board's duties. (NOTE: This provision shall not be deemed to prohibit communication with a neighbor or community member; however, such communication shall be limited to providing information about the design review process, meeting dates, general information, and gathering citizen input. All other inquiries should be referred to the Planning Department.)

- X. RESIGNATIONS: In the event a Board member can not complete his/her term of office, he/she shall submit a letter of resignation to the Board at least sixty (60) days prior to the date upon which his/her resignation takes effect. This will enable the Board to interview candidates for the vacant position.

The Board, upon the unanimous vote of the remainder of the Board, may request that a Board member resign from the Board if that member is not felt to be carrying out his/her duties with integrity, commitment and professionalism, and/or not abiding with the policies stated herein.

TAM DESIGN REVIEW BOARD ADMINISTRATION POLICIES

October 12, 1989

Page 6

XI. EFFECT: The provisions of this document shall take effect immediately upon their adoption by a majority of the Board.

Provision III shall take place immediately and shall apply equally to all current members of the Board, whether or not each has been officially appointed to a second term of office.

THE ABOVE DOCUMENT IS HEREBY ADOPTED AS AMENDED BY THE TAMALPAIS DESIGN REVIEW BOARD ON OCTOBER 12, 1989:

MOTION: Marchant

SECOND: Dedona

VOTE: Ayes: Alff Wiegel, Farnsworth, Richards, Busby
Dedona, Marchant

Noes: None

Absent: Davis, Zegart

Jan Alff Wiegel
Chairperson

Vice-Chair

Secretary

TAM DESIGN REVIEW BOARD

305 Bell Lane, Mill Valley, CA 94941

THE FOLLOWING IS AN AMENDMENT TO THE "ADMINISTRATIVE POLICIES" OF THE TAMALPAIS DESIGN REVIEW BOARD:

VIII: CONFLICT OF INTEREST AND ABSTENTIONS

This section, as contained in the policies adopted by the Board on October 12, 1989, is hereby deleted and the following section is substituted:

VIII. CONFLICT OF INTEREST AND ABSTENTIONS

A Board member shall be disqualified from voting on any motion or other matter pertaining to a proposed project or real property being considered by the Board if a conflict of interest exists. A Board member shall abstain from voting on any Application in which any of the following circumstances applies:

a) The Board member has a direct, indirect, or beneficial ownership interest in the proposed project or real property being considered by the Board.

b) The real property in which the Board member has an interest, or any part of that real property, is located within a 300' radius of the boundaries (or the proposed boundaries) of the proposed project or real property being considered by the Board.

c) The real property in which the Board member has an interest will receive new or substantially improved services as the result of construction, or improvements to, streets, water, sewer, storm drainage and/or similar facilities being considered by the Board.

d) The Board member has a leasehold interest in the proposed project or real property being considered.

e) The decision will change the legal allowable use of the property in which the Board member has a leasehold interest.

TAMALPAIS DESIGN REVIEW BOARD ADMINISTRATIVE POLICIES
Amendment to Section VIII

Page 2

f) It is reasonably foreseeable that the Board member will change the actual use of the property in which he/she has leasehold interest as a result of the decision.

g) The Board member owns, or has an interest in, a business located within a 300' radius of the boundaries (or the proposed boundaries) of the project being considered by the Board, or it is reasonably foreseeable that the proposed project will materially effect the Board member's business, either positively or negatively.

h) It is reasonably foreseeable that the proposed project being considered by the Board will affect (either positively or negatively) the use, development potential or income producing potential of the real property in which the Board member has an interest.

i) The Board member presently acts, or has acted in the past, as a representative of the Owner or Applicant of the proposed project or real property being considered by the Board, e.g., designer, architect, Owner's spokesperson, engineer, attorney, employee, etc.

When, in a Board member's opinion, he/she may have a potential conflict of interest, the Board member may request a vote of the entire Board to determine whether or not he/she should vote on the proposed project. Also, any Board member may request that the entire Board vote on the possibility of a conflict on interest of another Board member. If such a vote is taken and the majority of the Board deems that a conflict of interest exists, then that Board member shall be disqualified from voting on the proposed project.

No Board member shall make, participate in making, or in any way attempt to use his position to influence a County planning decision in which he/she knows, or has reason to know, he/she has a financial interest.

TAMALPAIS DESIGN REVIEW BOARD ADMINISTRATIVE POLICIES
Amendment to Section VIII

Page 3

Even though a Board member may be disqualified from voting on a proposed project under consideration, the Board member may address the Board as a whole about his/her concerns when the Board hears public comment on the project. Once the Board has concluded public comment, however, the disqualified Board member may not participate in any further Board discussions about the merits of the project.

All members abstaining from voting must be recorded on the Board's "Design Review Recommendations" for the proposed project being considered. The reason for the Board member's disqualification and abstention from voting must be recorded therein also.

EFFECT: The provisions contained herein shall take effect immediately upon formal adoption by the Tamalpais Design Review Board.

THE ABOVE DOCUMENT IS HEREBY ADOPTED, AS AMENDED, BY THE TAMALPAIS DESIGN REVIEW BOARD ON May 24th, 1990.

MOTION: Davis

SECOND: Richards

VOTE: Ayes: Alff Wiegel, Busby, Davis, Higgins,
Richards, Schonbrunn

Noes: None

Absent: Dedona, Farnsworth

Alff Wiegel
Chairperson

Joyce Farnsworth
Vice-Chairperson

Alfred Richards
Secretary

APPENDIX G
STEERING COMMITTEE ROSTER

Note: The steering committee roster has been deleted from the Appendices of the Plan, since the membership may change periodically.

**APPENDIX H
PARCELS WITH SUBDIVISION POTENTIAL**

PARCELS WITH SUBDIVISION POTENTIAL (1987)

Assessor's Parcel #	Current Zoning	Average Slope Less than 35%	Average Slope Greater than 35%	Maximum Density Allowed with Standard Zoning Plus Slope Ordinance	Equivalent Planned District Zoning
047-032-01	R-1		X	1 unit/acre	RSP-1.0
048-081-07	R-1		X	1 unit/acre	RSP-1.0
048-081-17	R-1:B-1		X	1 unit/acre	RSP-1.0
048-026-03	R-1:B-1	X		7.3 units/acre	RSP-7.3
048-031-24	R-1	X		5.8 units/acre	RSP-5.8
048-101-24	R-1	X		5.8 units/acre	RSP-5.8
048-101-25	R-1	X		5.8 units/acre	RSP-5.8
048-101-26	R-1	X		5.8 units/acre	RSP-5.8
048-071-09	R-1		X	1 unit/acre	RSP-1.0
048-181-18	R-1	X		5.8 units/acre	RSP-5.8
049-012-34	R-1:B-1		X	1 unit/acre	RSP-1.0
048-012-52	R-1:B-1		X	1 unit/acre	RSP-1.0
049-051-10	R-1:B-2		X	1 unit/acre	RSP-1.0
049-063-09	R-1		X	1 unit/acre	RSP-1.0
049-094-01	R-1:B-2		X	1 unit/acre	RSP-1.0
049-112-08	R-A:B-2		X	1 unit/acre	RSP-1.0
049-113-01	R-A:B-2		X	1 unit/acre	RSP-1.0
049-202-14	R-1	X		5.8 units/acre	RMP-5.8
049-228-01	R-1:B-1		X	1 unit/acre	RSP-1.0
049-231-09	R-1:B-1		X	1 unit/acre	RSP-1.0
049-225-04	R-1:B-1		X	1 unit/acre	RSP-1.0
049-222-02	R-1:B-1		X	1 unit/acre	RSP-1.0
049-222-03	R-1:B-1		X	1 unit/acre	RSP-1.0
049-214-16	R-1:B-1		X	1 unit/acre	RSP-1.0
049-202-21	R-1	X		5.8 units/acre	RSP-5.8
049-204-01	R-1			1 unit/acre	RSP-1.0
049-171-32	R-1			1 unit/acre	RSP-1.0
050-081-04	R-A:B-2	X		5.8 units/acre	RSP-5.8
050-081-40	R-1	X		5.8 units/acre	RSP-5.8
052-180-04	R-3			5.8 units/acre	RMPC
052-182-02	R-3			5.8 units/acre	RMPC
052-182-03	R-3			5.8 units/acre	RMPC
052-211-01	R-A:B-1		X	1 unit/acre	RMPC-1
052-211-02	R-A:B-1		X	1 unit/acre	RMPC-1

* Application of the County Slope Ordinance will determine maximum permitted density.

APPENDIX I

TAM JUNCTION DESIGN GUIDELINES

TAM JUNCTION
DESIGN GUIDELINES

Prepared By

The Marin County Planning Department

EDAW, Inc.

July, 1989

TABLE OF CONTENTS

1. INTRODUCTION

- 1.1 Purpose and Intent 1
- 1.2 Location 2
- 1.3 Related County Policies and Regulations 2
- 1.4 Related Tam Area Community Plan
Goals, Objectives and Policies 4
- 1.5 Land Use Plan Concept 7

2. SITE PLANNING AND DEVELOPMENT GUIDELINES

- 2.1 Land Use Planning and Zoning 1
- 2.2 Circulation and Parking 3
- 2.3 Site Planning Guidelines 5

3. LANDSCAPE GUIDELINES

- 3-1 Overall Landscape Concept 1
- 3-2 Edges 2
- 3-3 Streetscape 3
- 3-4 Gateways 4
- 3-5 Open Space 6
- 3-6 Urban Design Features 7

4. SIGNAGE AND LIGHTING

- 4.1 General Signage Guidelines 1
- 4.2 General Lighting Guidelines 6
- 4.3 Utilities 8

APPENDICES

- A. LANDSCAPE MATERIALS**
- B. RELATED COUNTY POLICIES AND REGULATIONS**

INTRODUCTION

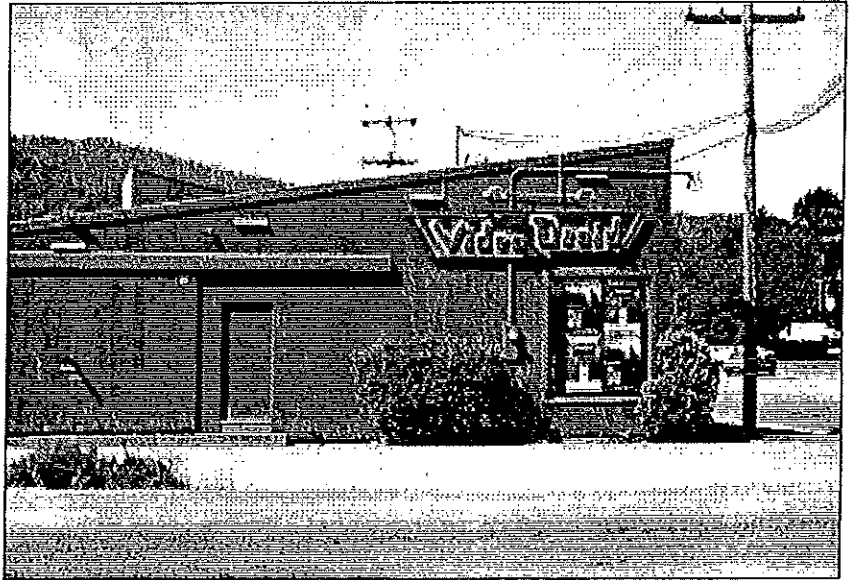


INTRODUCTION

1.1 PURPOSE AND INTENT

These design guidelines and development standards have been prepared specifically for the Tam Junction Area of the Tamalpais Planning Area. These guidelines are intended to provide a set of parameters, both fixed and flexible, which will be used by the community's Design Review Board, County staff and property owners and developers when they plan, propose and evaluate applications for changes in Tam Junction. These guidelines establish the framework for a level of project design that reflects the preference of the existing community while addressing issues that are both general to the entire Junction and specific for special places. These guidelines express the preferred architectural and landscape character for future development of office, retail and residential land uses in the Junction area. These architectural and landscape guidelines, when combined with planning and engineering, form the urban design of the Junction. It is the intent of these guidelines to identify the community features, such as landscaping, open space, signage, lighting street furniture, parking and circulation that, through quality design and construction, will create the image and character preferred by the Tam Valley Community.

These design guidelines and development standards are intended to establish criteria for the entire Junction to ensure a cohesive environment. Basic to this purpose is the assumption that each development, or redevelopment, proposal should relate to the overall image of the Junction and contribute to improving the quality of the area. It is not the intention of these guidelines to limit or inhibit innovative design, but rather to provide clear direction while establishing minimum standards for most situations, at the site specific level, relevant design guidelines and individual projects. Similar uses, such as retail or office, will have common



Commercial use adjacent to the gateway area of Tam Junction

elements, such as signage or landscape programs. These elements will reflect the fixed guidelines, those not subject to interpretation by the Design Review Board. However, to allow for separate identities for each individual project, differences in product type, use and tenant preferences, are recognized by the guidelines that allow flexibility and will require interpretation during the review process.

During the development of the Tamalpais Area Community Plan and the specific planning for the Tam Junction, the Steering Committee identified the characteristics and special features of the Junction recognized by the community and suggested the overall character or theme for the Junction as it should be in the future. The Tam Junction should be the focal point of the community, reflecting a scale and intensity of development that recognized a pedestrian and bicycle-oriented circulation system similar to the town centers of Mill Valley or San Anselmo. The Junction should also reflect the importance of its function and image as the gateway

INTRODUCTION

to Tam Valley, Mill Valley and the Golden Gate National Recreation Area.

In summary, the goals of these design guidelines and development standards are as follows:

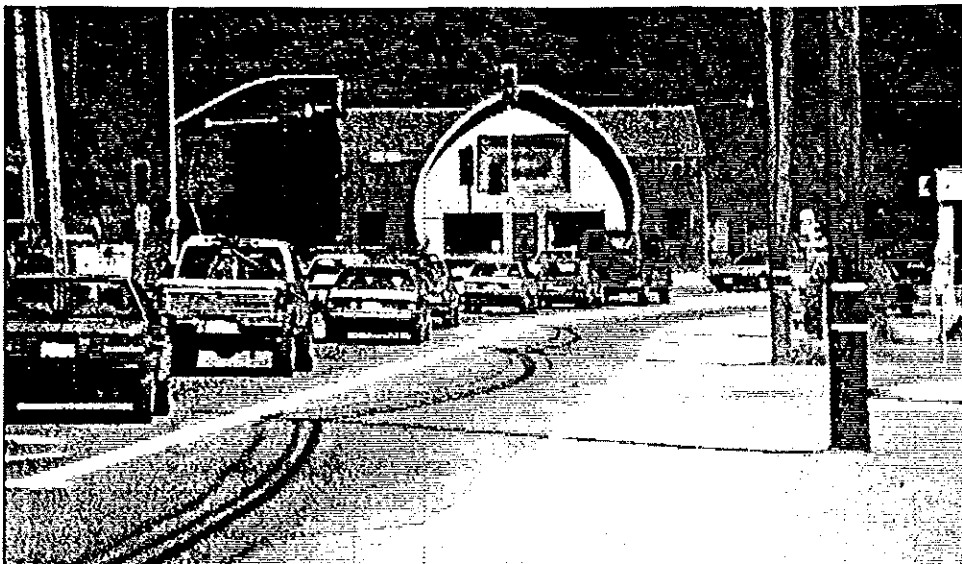
- To provide the Tamalpais Area community with the necessary assurances that future development projects will conform to the goals and objectives of the community Plan and contribute the level of quality desired for Tam Junction;
- To serve as design and development criteria for use by planners, architects and building designers, landscape architects, engineers, property owners and developers;
- To provide guidance to the Design Review Board and the County Planning Commission and the Board of Supervisors when reviewing development and redevelopment projects within the Tam Junction area; and
- To provide a viable framework and clear set of implementation strategies and measurable standards which will achieve the community's goals and objectives, while encouraging the creativity of qualified design professionals and property developers.

1.2 LOCATION

The Tam Junction area is a 20.56 acre located around the intersection of the Shoreline Highway (Highway 1) and Almonte Boulevard at the entrance of Tamalpais Valley adjacent to the marshes and wetlands of Richardson Bay. The Junction also serves as the gateway to Mill Valley by way of Almonte Boulevard and the Golden Gate National Recreation Area via Highway 101. From the north, access is from the City of Mill Valley along Almonte Boulevard. Figure 1-1, the Vicinity Map, shows Tam Junction and its relationship to the surrounding area and the regional circulation network.

1.3 RELATED COUNTY POLICIES AND REGULATIONS

The four goals established for the 1982 Countywide Plan are general in nature and provide no specific guidance for Tam Junction. Similarly, none of the policies contained in the Countywide Plan include a specific reference to the Junction area. However, the intent of many of the policies can be interpreted as having an application to Tam Junction. To summarize the most important policies that impact the Junction areas, those policies specifically dealing with open space preservation, Stream



Typical warm weather traffic on Shoreline Highway

APP-I-IN.DOC

INTRODUCTION

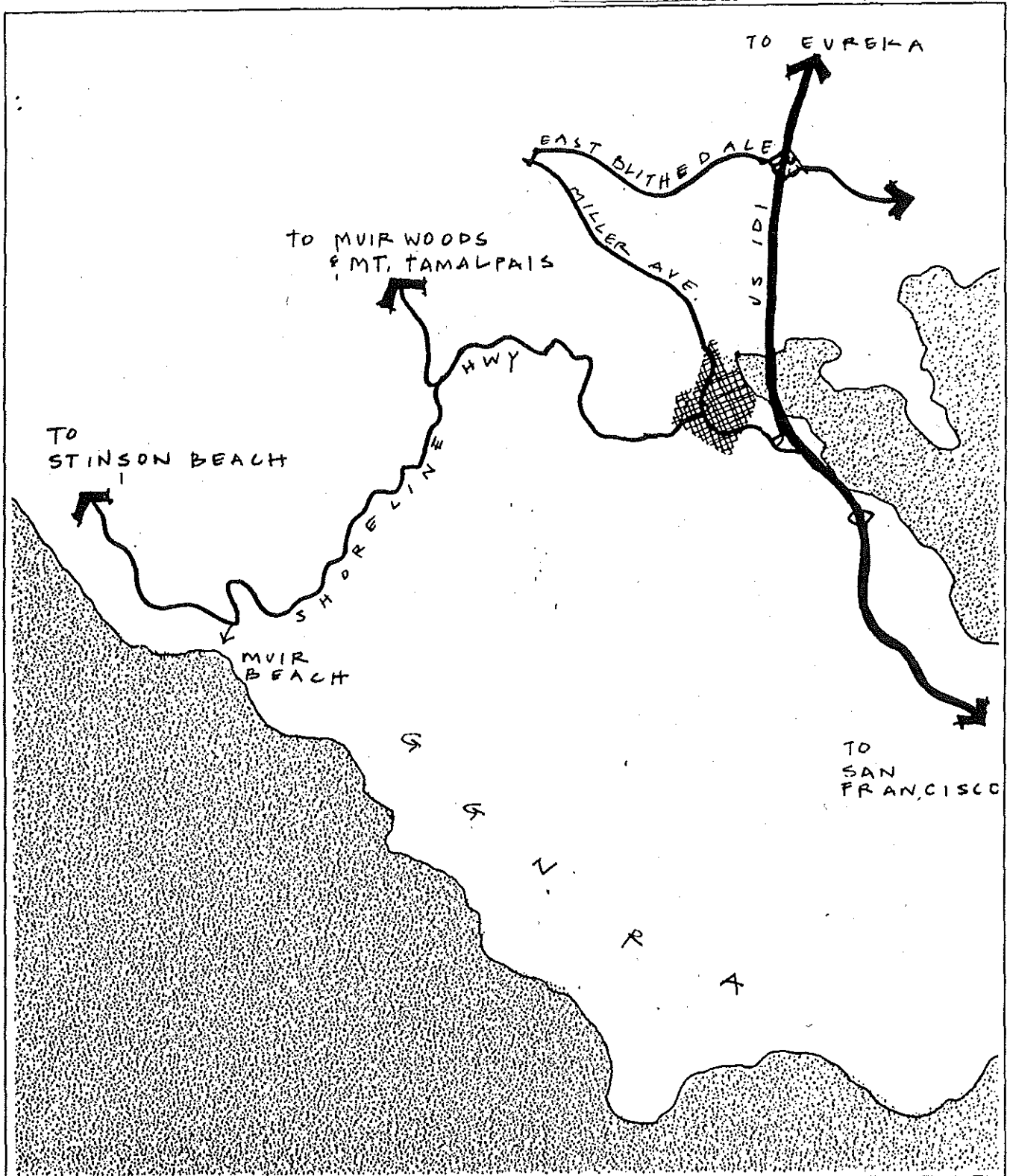


FIGURE 1-1 VICINITY MAP

INTRODUCTION

Conservation Areas, marsh enhancement and public access are consistent with the Community's desire to create a major open space element in the area east of the Shoreline Highway. Other policies, specifically relating to the concentration of development, including infill development, are consistent with the Improvement Plan's main objective to create a village core in the Junction. The Community Development Policies suggest that housing should be integrated into the commercial areas. This is also consistent with the objectives expressed by the community. The Economic Development Policies are also reflected by the Improvement Plan's Village core concept.

1.4 RELATED TAM AREA COMMUNITY PLAN GOALS, OBJECTIVES AND POLICIES

The two primary goals of the Community Plan set the basis for planning in the Junction Area. These goals are as follows:

1. Maintain the semi-rural character of the area as defined by its small town residential and commercial setting and the quality of its natural environment.
2. Recognize the distinctiveness and interdependence of the neighborhood and geographic areas.

The first goal relates directly to the Junction, the second is more indirectly related.

The objectives and policies of the Community Plan include programs that are specific to the Junction area and the adjacent neighborhoods. These objectives and policies can be related to the Junction in a general way while several programs directly effect the existing and proposed land uses. The following is a list of the programs that relate specifically to the Junction and comments related to the relevance of each.

LAND USE

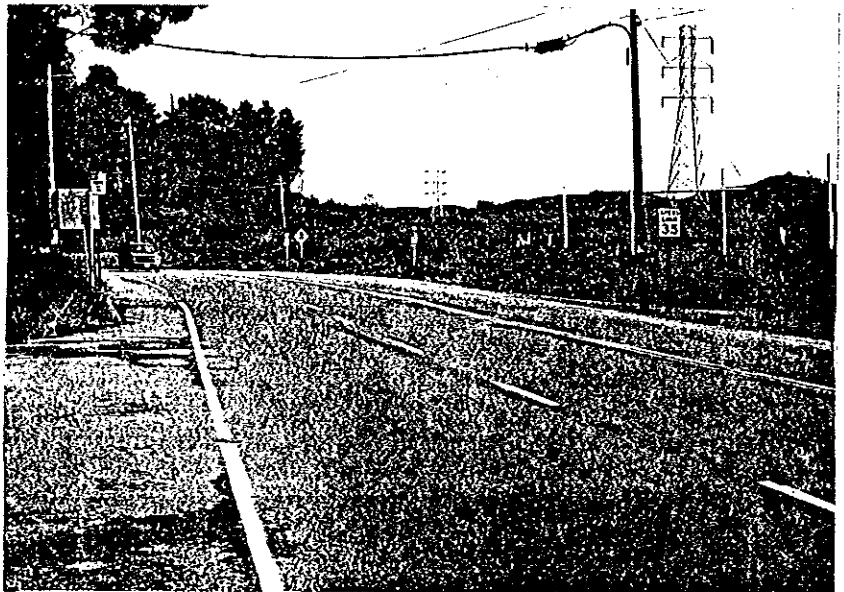
RESIDENTIAL

Policy LU-2 includes several implementation programs that are specific to the Junction area.

Program LU2.3(a): The east side of Almonte Boulevard extending from Tam Junction to the Mill Valley City limits is highly visible and associated with the Bothin Marsh. Access to this parcel from Almonte Boulevard, particularly from the area north of Helen Avenue, is difficult because of high traffic volumes and constrained sight distances due to the geometry of the existing roadway. This parcel should be rezoned BFC-RSP in order to ensure proper design review.

Comments: The recommended zoning change for this parcel is consistent with the goal of the Junction Improvement Plan to create an area of significant open space between Shoreline Highway and Almonte Boulevard and the existing Bothin Marsh.

Eastside of Almonte Boulevard
opposite Helen Avenue



INTRODUCTION

Program LU-2.3(b): Assessor's Parcel #051-262-12, 13, 34, 35 located at the intersection Gibson Avenue and Shoreline Highway currently have some single family homes and do not meet minimum lot size requirements of their R:1 zoning designation. Furthermore, they are located in an area of extreme traffic congestion and their only available access is directly onto Shoreline Highway which causes additional traffic congestion. It is recommended that these parcels be rezoned to be compatible with their lot size (approximately 5,000 square feet each) and with an RSP designation (RSP-8.7) in order to ensure proper design review of building placement and access to Shoreline Highway.

Comments: One of the main objectives of the Junction Plan is to improve the flow of traffic through the Junction in both directions along Shoreline Highway. This program helps implement the intent of the Improvement Plan.

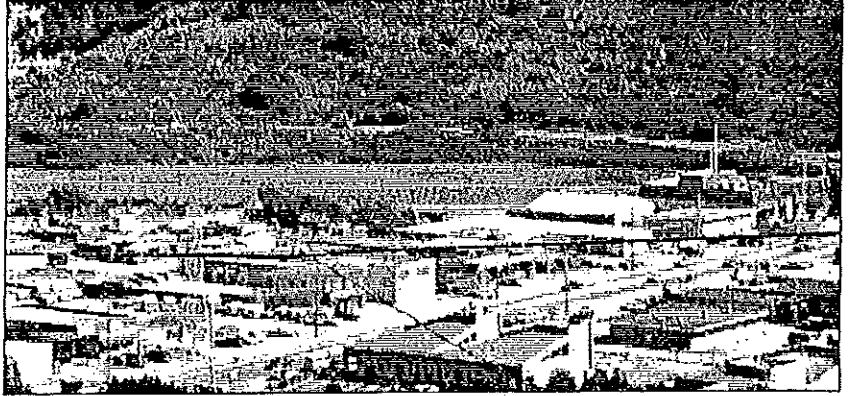
COMMERCIAL AREAS

Policy C-1: Improve the social, economic and environmental viability of the Tam Junction Commercial Area while at the same time open vistas and pedestrian access to the Bothin Marsh.

Comments: The expanded development guidelines conform to this policy to improve the viability of the Tam Junction Commercial Area.

Policy C-2: Maintain the mix of uses and preserve and promote resident serving uses along the west side of Shoreline Highway between Tam Junction and Coyote Creek and along the north side of Highway 1.

Comments: The Junction Improvement Plan includes several specific guidelines that relate to the commercial uses in the junction.



Mixed uses along Shoreline

Policy C-3: Improve the appearance of community business area.

Comments: The signage and landscaping guidelines included in the Junction Improvement Plan are directly related to the intent of this policy.

Policy C-4: Rely on existing commercial centers outside the community for major goods and services.

Comments: The uses recommended by the Junction Improvement Plan are consistent with this policy and reflect the community's preference for a pedestrian scale village center.

Policy C-5: When funding sources become available, the land use and capital improvement recommendations presented in the Plan for Tam Junction shall be implemented.

Comments: The Junction Improvement Plan conforms to this policy.

INTRODUCTION

Policy C-6: The recommendations of the Steering Committee for land use, building FAR, height and design guidelines for the Shoreline Area shall be followed during review of development applications.

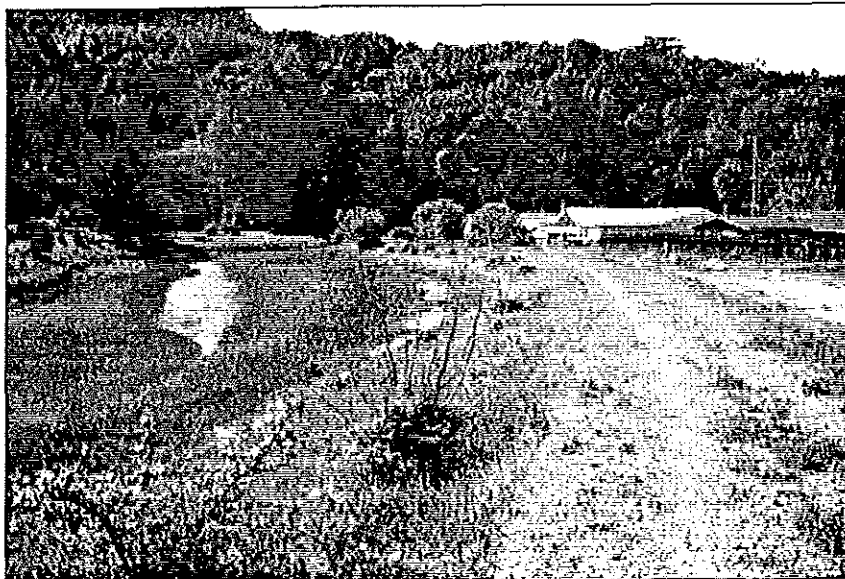
Comments: The design guidelines included in the Junction Improvement Plan include the recommendations of the Steering Committee. These guidelines are intended to be used by the Design Review Board, the Planning Commission and the Board of Supervisors to evaluate proposed development projects.

Policy C-8: This Plan recommends further expansion of strip commercial through redevelopment or conversion of residential uses to commercial uses be prohibited. The Plan also recommends that the existing level of building intensity be maintained in order to focus primary commercial development in the Tam Junction area.

Comments: The village core concept for the Junction area is consistent with this policy. Program C-8-1 specifies a zoning designation (RMPC) that encourages a mix of uses, commercial and residential, and requires a master plan for proposed development. The Junction Improvement Plan design guidelines are an expansion of the design guidelines included in the Community Plan. The mix of uses shall approximate a mix of one to one residential to commercial, including two story buildings with residential uses above commercial uses, and garden apartments, where appropriate.

OPEN SPACE

The Junction Improvement Plan conforms to the objectives of the open space element of the Community Plan, particularly those objectives related to the marshlands of Richardson Bay and Coyote Creek.



Coyote Creek and maintenance road

Policy OS-1: The County should require proper management for the long-term protection and diversity of native vegetation and habitats, throughout the developed portions of the community as well as on undeveloped open space lands.

Comments: The Junction Plan encourages long-term protection of the native vegetation and wildlife habitats by enhancing the marsh and Coyote Creek environments.

Policy OS-2: The County shall regulate new or altered development and vegetation removal to ensure that site preparation and construction do not cause or contribute to erosion or slope failure, with resulting loss of life or properties, downstream flooding, or siltation of wetlands. Development shall be located in the most accessible, least environmentally sensitive and most geographically stable area or areas of a development site.

Comments: The Junction Improvement Plan conforms to this policy, particularly through the programs that relate to the stream corridor.

INTRODUCTION

Policy OS-3: In view of the acknowledged regional values of the waters, marshes and shoreline areas of Upper Richardson Bay, the County shall make a comprehensive effort to maintain and enhance long-term visual and physical access to Tamalpais Preserve (known locally as Bothin Marsh) in order to protect the water-related habitat of the area and to establish it as a major gateway to the Tamalpais Area.

Comments: The concept for the village core area includes recommendations for the enhancement of Tamalpais Preserve and the creation of a major open space element along the east side of Shoreline Highway. The Junction Improvement Plan conforms to the programs related to this policy.

1.5 LAND USE CONCEPT

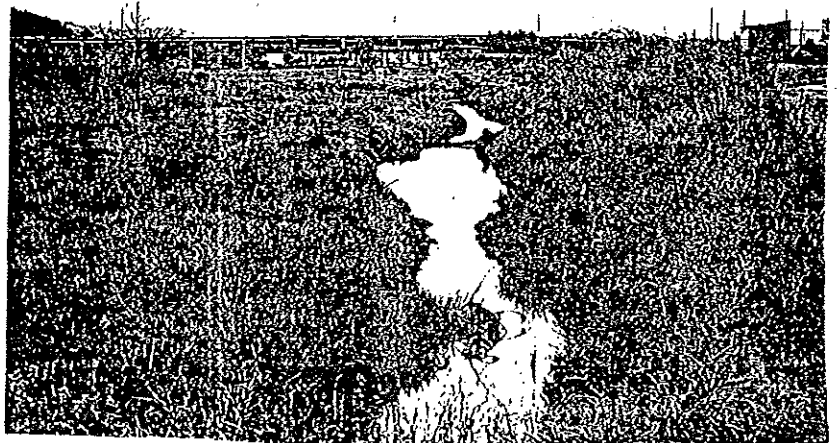
As part of the process used to develop the Community Plan for the Tam Valley area, planning work was included that was specific to the Tam Junction area. The initial planning phase included the development and evaluation of a series of alternative land use and circulation concepts, resulting in the selection of a preferred alternative by the Steering Committee after a series of community workshops and meetings. This preferred alternative, referred to during the process as "Alternative A" is based on the concept that the area can become a concentrated village core providing a centralized focal point for the Valley while reflecting a mix and balance of uses. The Junction area, being split by the existing Shoreline Highway, has two components, the east and west sides. The concept for the east side includes a reorganization of the area to create open space, including the restoration and enhancement of the historic marsh and wetland areas, while allowing the redevelopment of retail, office and service commercial uses. The west side would continue to have a variety of land uses, but those uses will be planned and developed as part of a cohesive whole as a village

center. The existing retail, office, service, commercial uses could be enhanced by other compatible uses, such as residential, that are consistent with the village core concept. Uses that do not contribute to the active life, both daytime and nighttime, of a village core are not included in the land use concept.

The circulation concept utilizes the existing roadway network and calls for specific projects such as street widening, turning lane additions, and parking improvements that will resolve some of the traffic conflicts now experienced by the community.

Figure 1-2 indicates the Concept Plan for the Junction.

Tamalpais Preserve



INTRODUCTION

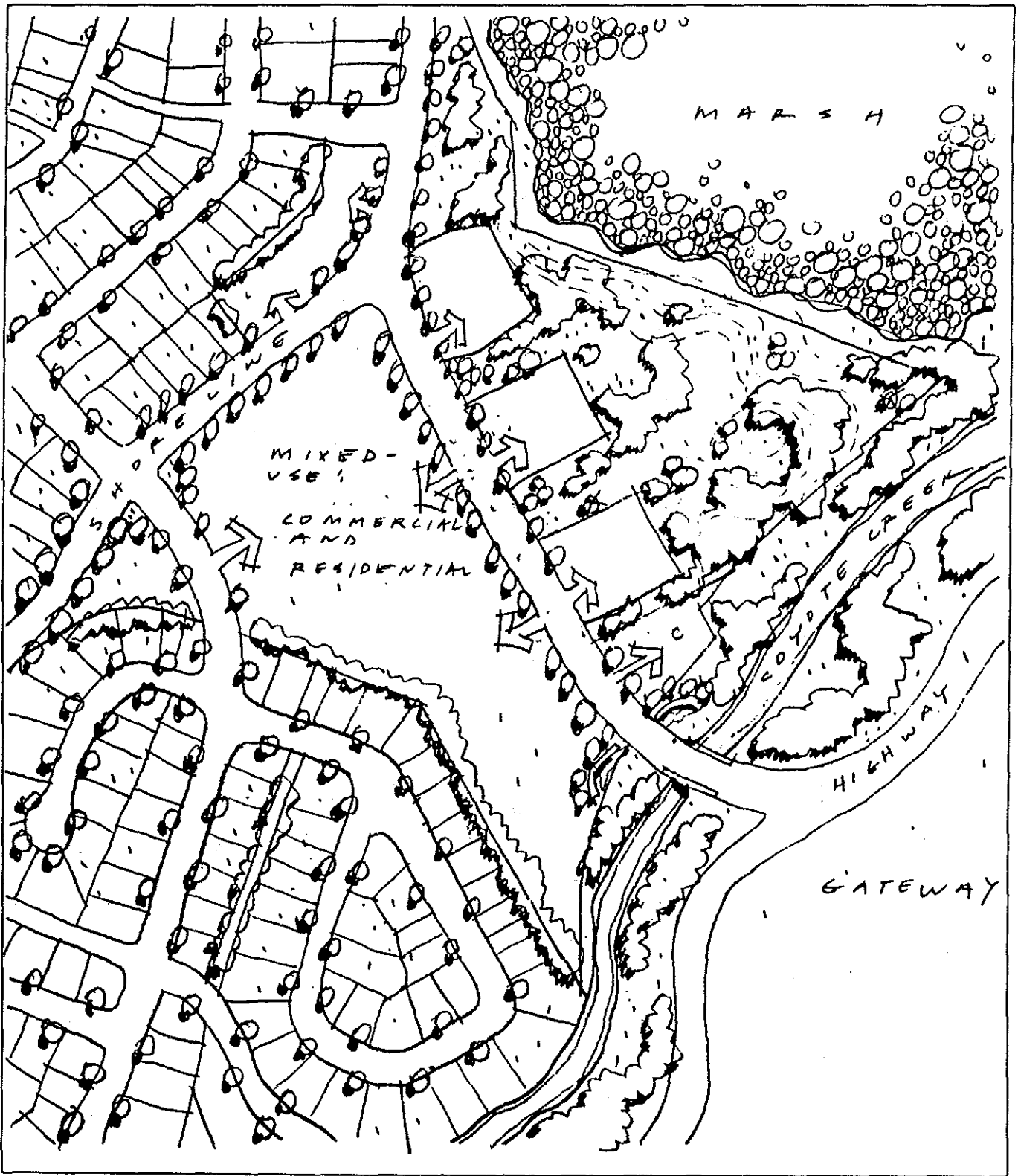
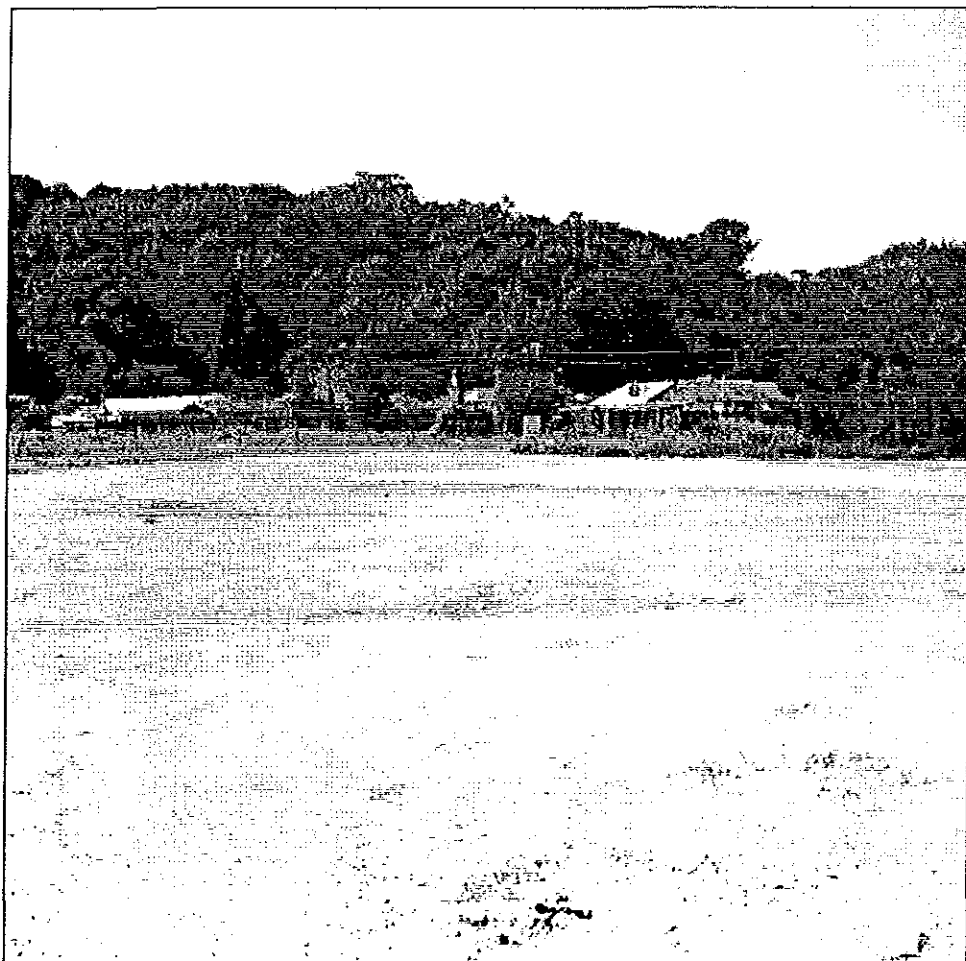


FIGURE 1-2
LAND USE CONCEPT PLAN

APP-1-IN.DOC

SITE PLANNING AND DEVELOPMENT GUIDELINES



SITE PLANNING & DEVELOPMENT GUIDELINES

2.1 LAND USE PLANNING AND ZONING

LAND USE PLANNING

The village core concept establishes a combination of compatible uses that reflect the community's goals. The preferred businesses are as follows:

- Antique stores
- Appliance repair
- Architectural services
- Art galleries
- Artist supply
- Athletic supply stores
- Auditorial services
- Automobile service, repair and supply (west side only)
- Bicycle and motorcycle shops
- Bookstores
- Building materials supply
- Cleaners and Laundries
- Clothing and shoe stores
- Consulting services
- Community meeting rooms
- County library branch
- Drug stores/Pharmacies
- Emergency services (fire, police)
- Engineering/Architecture/Landscape
- Fabric shops
- Gift shops
- Hardware stores
- Hobby shops
- Insurance offices
- Jewelry stores
- Kitchen supply stores
- Legal services
- Liquor stores
- Local Government offices
- Markets and grocery stores
- Medical, dental, optical, and auditorial and Health related services
- Music stores
- Nurseries
- Paint stores
- Pet stores
- Photography stores
- Real estate services

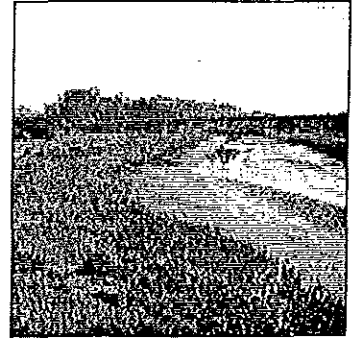
- Residential
- Restaurants/food services, cafes, bars, delicatessens
- Shoe repair services
- Stationery stores
- Television and radio sales and repair
- Variety stores
- Veterinary services
- Video stores

A proliferation of similar uses shall be prohibited. Uses that do not contribute to the village core concept, such as warehousing, storage or manufacturing, are considered inappropriate for the Tam Junction area.

Tam Junction is perceived as a balanced village core and gateway for the Tam Valley Community, as well as the gateway to both Mill Valley and West Marin. The mix and balance of compatible uses include retail, office and residential opportunities that will contribute to the quality of life preferred by the community as expressed by the Tam Area Community Plan. The most intensive development will be concentrated on the west side of the Junction area between Shoreline Highway and Flamingo Road. Development will occur in an orderly manner, reflecting both the desires of the community and the goals of individual property owners as they react to market demands.

Zoning

The long range goals of the Tamalpais Community Plan, to maintain the semi-rural character of the area to recognize the unique character of the existing environment (page 1-4) and Policy OS-1 (Page 1-6) directly influence the proposed zoning designation for the Junction.



Coyote Creek leading to Richardson Bay

SITE PLANNING & DEVELOPMENT GUIDELINES

The current zoning for the Junction is R1:B-1 (Single Family Residential), R3 (Multifamily Residential), RMPC (Mixed Commercial/Office, Residential) CP (Planned Commercial) and OA (Open Space). These zoning categories allow the uses compatible with the land use concept.

-Residential units in the Junction should be clustered as attached townhouses, condominiums or apartments. Freestanding, single family residential units are not appropriate in the village core area.

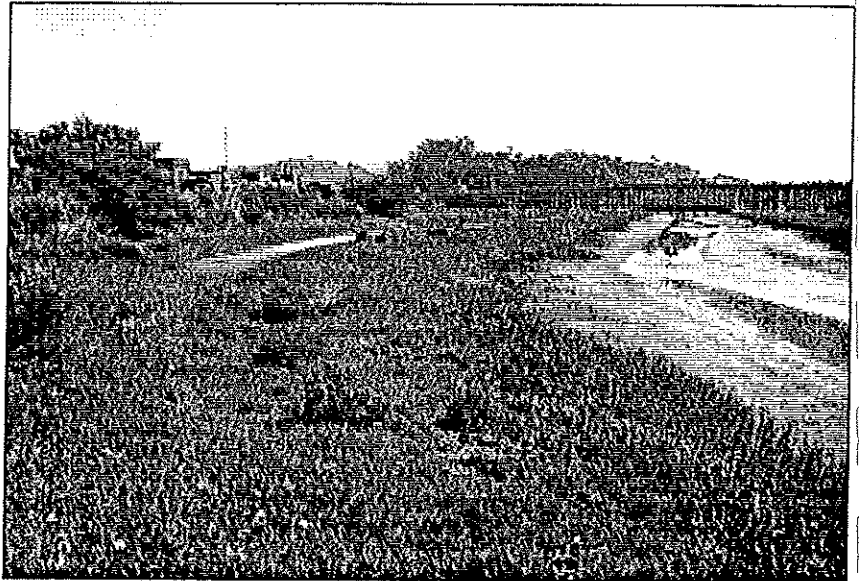
-Residential units should be incorporated in the design of commercial uses, using an architectural character that is part of or compatible with the surrounding buildings.

-Residential uses in the village core area should include smaller, affordable units where affordable.

The zoning proposed by this improvement plan indicates that a large portion of the east side of the Junction should be zoned OA and RMPC, allowing those uses compatible with the village core concept while including an emphasis on open space. The west side of the area is to be zoned BFC-RMPC.

2.2 CIRCULATION AND PARKING

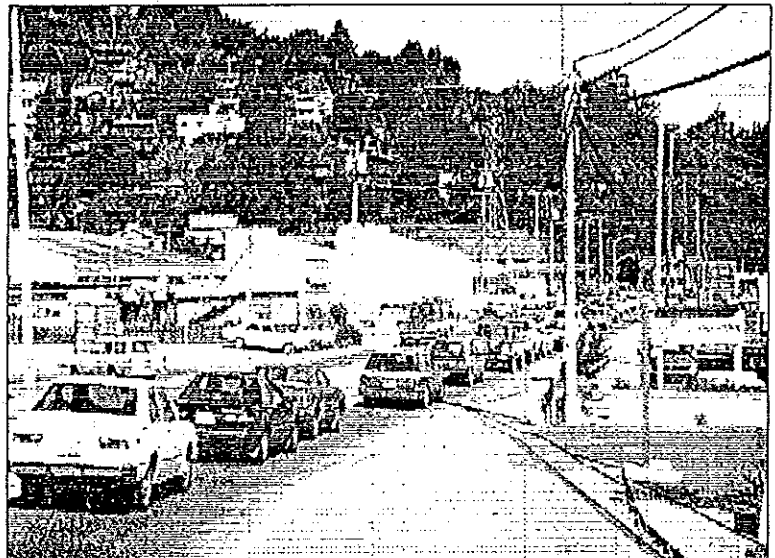
The existing roadway network provides the framework for the Junction. Shoreline Highway, Almonte Boulevard and Flamingo Road are fixed and can only be modified in a limited number of places along those roadways to improve the flow of traffic. The existing patterns of uses on both sides of the Junction has established a very inefficient network of driveways, parking lanes and informal roads. As the Junction is redeveloped, this internal system will change considerably to accommodate the new configuration of the village core.



Coyote Creek

The proposed improvements to Shoreline and Flamingo are intended to improve the flow of traffic, specifically during normal peak hours and seasonal peak hours. These improvements will benefit both the Tam Area community and Mill Valley when the primary gateway, the intersection of Shoreline Highway and Almonte Boulevard is improved. Figure 2-4 indicates the proposed circulation plan and improvements.

Shoreline Highway, midday traffic on hot weekend day



SITE PLANNING & DEVELOPMENT GUIDELINES

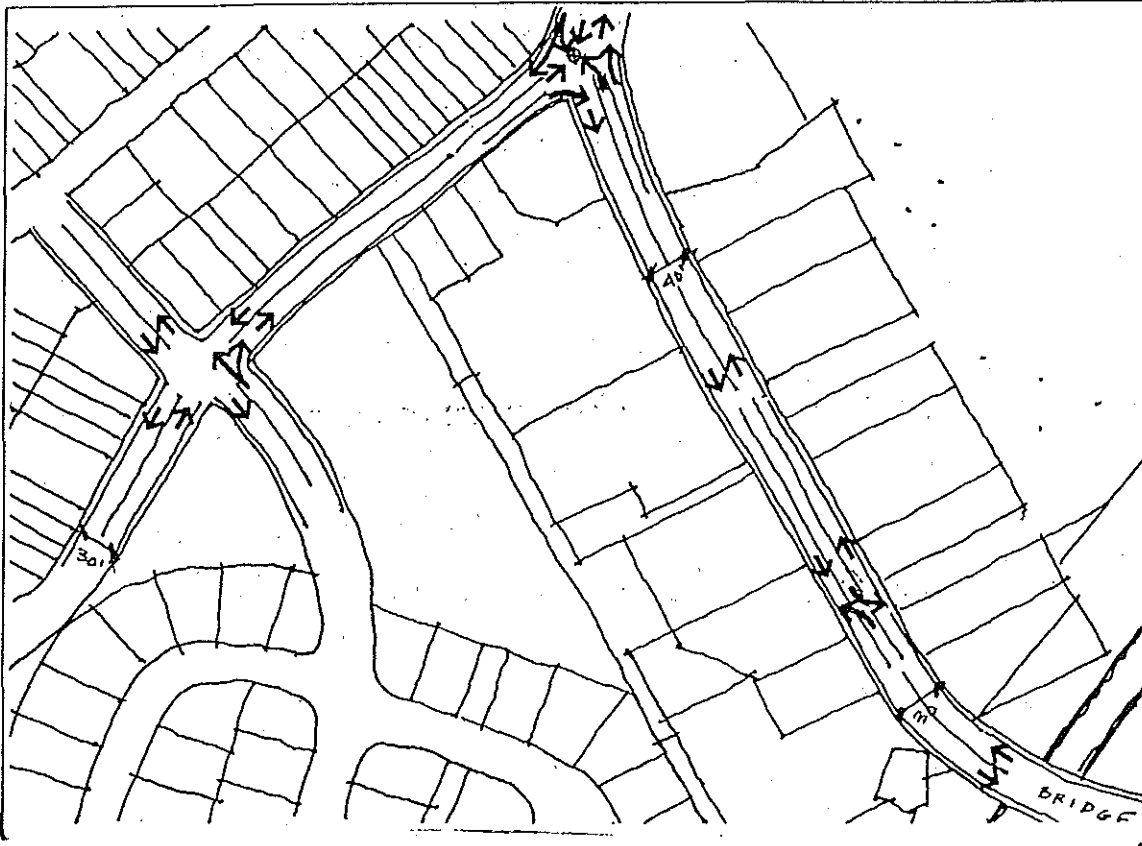
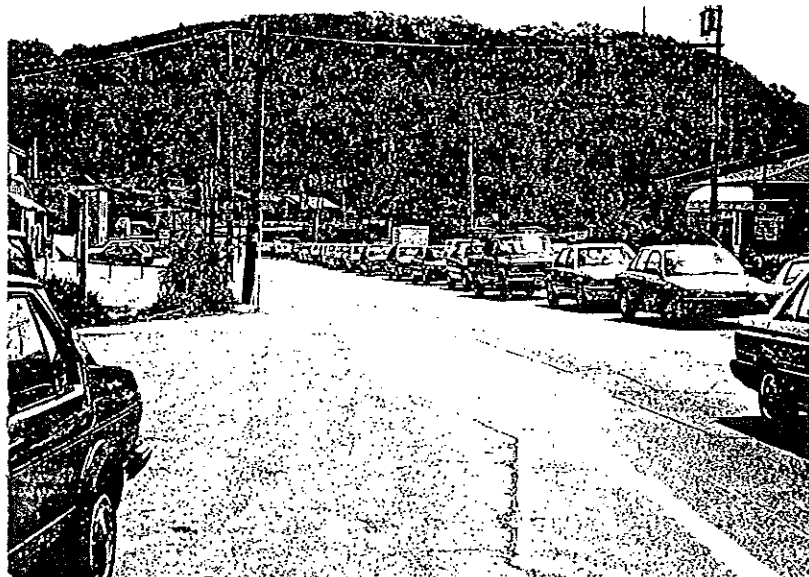


FIGURE 2-4 - PROPOSED CIRCULATION CHANGES

Note: This figure does not depict final recommendation. Please see Revised Community Plan.



Cars waiting to turn left at Tam Junction

SITE PLANNING & DEVELOPMENT GUIDELINES

The 60 foot right-of-way for shoreline between the bridge and the signal light shall include two through lanes in either direction with a two-way left turn lane in the middle.

The two-way left turn lane would terminate approximately 200 feet from the signalled intersection and be replaced by a left turn only lane allowing two left turn lanes for the West Marin direction.

In order to improve the segment of Shoreline between the Shoreline/Almonte intersection and Flamingo Road, the width of the paved roadway must be increased to allow two west-bound lanes, one through east-bound lane and one right turn only lane. A left turn only lane is necessary at Flamingo Road to improve the flow of traffic turning from Shoreline Flamingo Road.

The Shoreline Highway/Flamingo Road intersection would also include a left turn and right turn lane onto Shoreline Highway from Flamingo Road. Direct access to Gibson from Flamingo across Shoreline would be restricted by a raised median in the center of the intersection.

The current location of the access to the Cala Foods parking area is too close to the Flamingo Road/ Shoreline Highway intersection and vehicles turning left into the parking area or left onto shoreline from Flamingo Road create a delay in traffic flow under certain conditions. Future plans for this area shall not include any access closer than 50 feet from the intersection.

Landscape materials shall be placed in these areas so as not to conflict with sight lines for vehicles of any kind.

2.3 SITE PLANNING GUIDELINES

2.3.1 Site Planning

The site planning guidelines described below apply to the Tam Junction area, on both sides of the Shoreline Highway. The County's Zoning



Existing Flamingo Road setback

Ordinance is incorporated into this section of the guidelines by reference. Where these guidelines conflict with the existing zoning regulations, the more restrictive shall apply.

- When development of any kind is proposed for the so-called "Cala property", Assessor's Parcel number 051-051-01, between Shoreline Highway and Flamingo Road, a master plan for the entire contiguous ownership shall be required as one of the conditions of approval. Existing uses or buildings that may remain in existence for a term longer than 20 years may be incorporated into the master plan as existing.

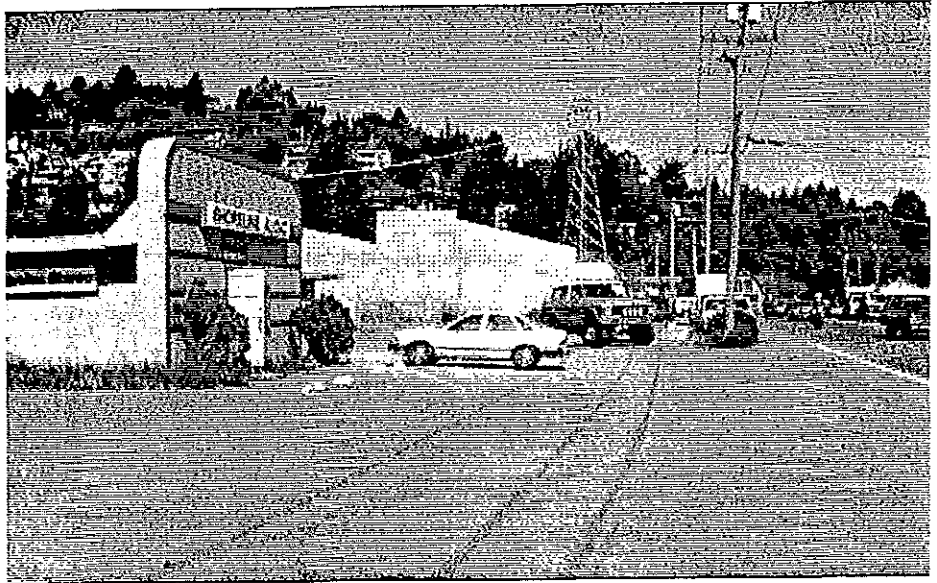


Existing setback along Shoreline

- When development of any kind is proposed for the so-called Martin Brother's Triangle area of the Junction, Assessor's Parcel Number 052-052-39, a master plan and development plan for the contiguous ownership between the Shoreline Highway and the marsh from Rosemont Avenue to the Coyote Creek bridge shall be required prior to project approval. Existing uses or buildings that may remain in existence for a term longer than 20 years may be incorporated into the

SITE PLANNING & DEVELOPMENT GUIDELINES

Existing parking area along Shoreline



incorporated into the master plan as such. Any development in this area is subject to the County's seismic Safety Element.

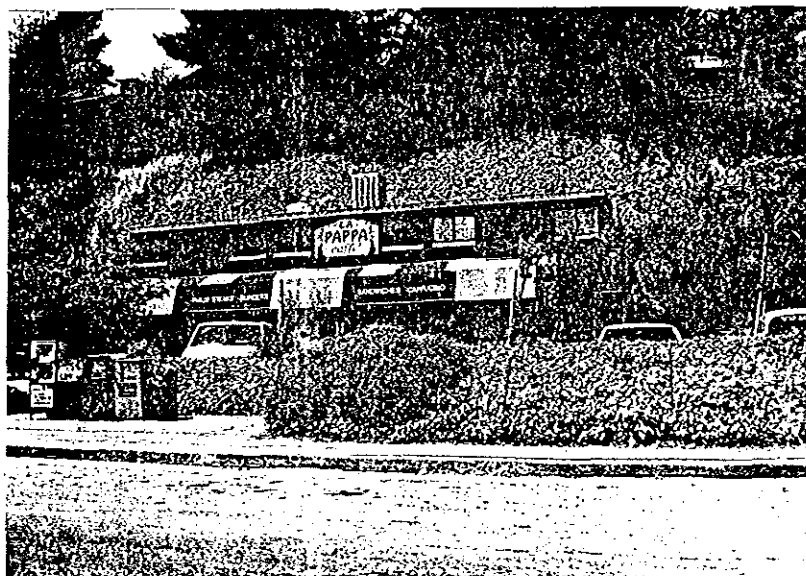
- Buildings will be set back 15 to 30 feet from the Shoreline Highway right-of-way to provide adequate landscaping and relief from the roadway edge. Where appropriate the landscape easement shall include pedestrian and bicycle paths. Parking areas should be separated from the walkways along Shoreline and Flamingo by landscaped areas. Landscaped berms, for the purpose of screening parking areas from the pedestrian areas are recommended along the Shoreline Highway and Flamingo Road edges of the Junction area.
- The height limit for the area east of Shoreline Highway shall be 15 feet, measured above finished grade. The height limit on the west side shall be two stories, or 30 feet, measured from the natural grade.
- The Floor to area ratio (FAR) for the area east of Shoreline Highway shall be .35. The FAR for the area of west of Shoreline shall range from .35 to .40, which the higher FAR being approved if the proposed development exhibits exceptional design and public benefit.

Landscaped Setbacks

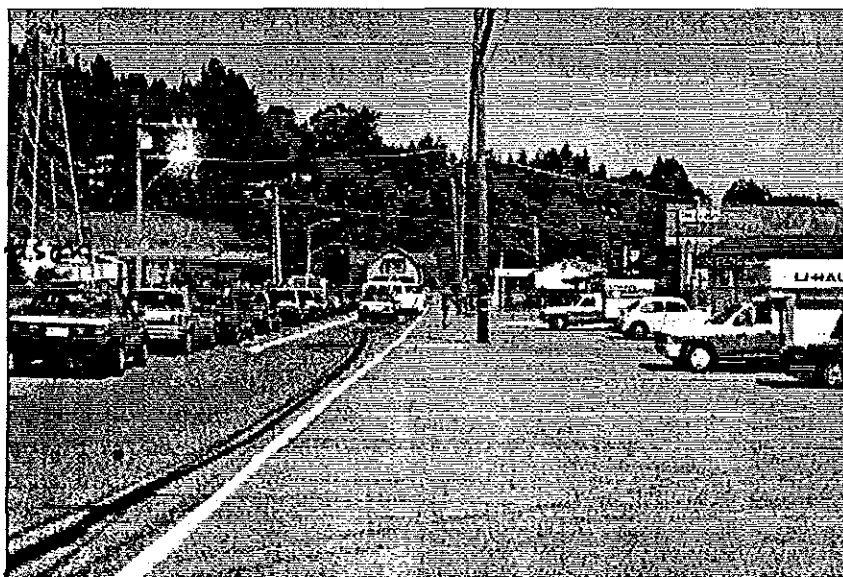


SITE PLANNING & DEVELOPMENT GUIDELINES

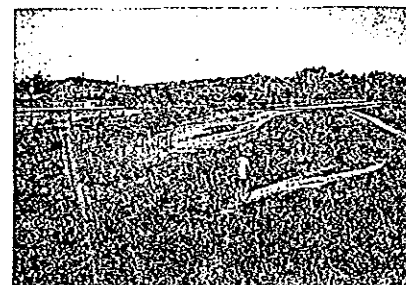
- Buildings shall be easily accessible by handicapped/disabled individuals. Shade, seating, lighting, shelter from the rain, and other comfort features should be incorporated into the design of each project.
- Buildings should be sited with consideration for the adjacent buildings and landscape. This is particularly important in the area adjacent to the existing residential area along Cardinal Court. New structures should reflect the village core concept in scale and detail.
- Building placement on adjoining parcels along Shoreline Highway or Flamingo Road should be varied to avoid creating a single monotonous blockface or streetscape. Grouped buildings should have setbacks that vary in depth from the roadway. Visual access to the marsh area and Richardson Bay shall be preserved.
- Commercial buildings should be clustered to create plazas and other usable public oriented spaces for special events and visual interest.
- Landscaping should be used to break up otherwise uninterrupted building mass, frame views to the marsh or the hills or interrelate with adjacent development.
- The visual impact of parking areas should be reduced by use of landscaping in the form of hedges, planted berms, islands and fingers. Berms should only be used in the internal areas of the Junction commercial zones and not along the main roadways.
- Divider islands four to five feet in width planted with low growing shrubs or dover shall be installed between and at the ends of parking rows. (Sketch) Trees will be planted at a rate of one tree for every five parking stalls, to be located in a planter. Pedestrian walk through areas, using paving elements, shall be installed between and at the ends of the parking rows.



Parking screened by berms



Parking area on east side of Shoreline



Visual access to marsh area and Richardson Bay

SITE PLANNING & DEVELOPMENT GUIDELINES

- Where parking areas are constrained and parking must be configured back to back without a landscape strip, tree pockets will be provided every second or third parking space as a six foot by six foot planter pit in the paved area.
- Parking areas shall include spaces for vehicles operated by handicapped individuals. These spaces shall be identified by the appropriate markings and symbols.
- All parking spaces shall be clearly defined. Paint, stone, brick, concrete or similar materials may be used to mark parking spaces.
- High quality roofing materials, such as sealed non-reflective metal, clay or concrete tile and concrete or asphalt built-up composites are recommended. Flat roofs should be avoided.

ARCHITECTURE:

The architecture of the Junction area is an essential element of the village core concept. It should reflect the transition between urban and rural. Extremely modern as well as extremely rustic architecture is not consistent with the village concept. In general, the architectural style should reflect the quality of other village centers, such as Mill Valley or San Anselmo, in Marin County. Building forms, limited to two stories, should be more horizontal than vertical. Occasional vertical elements, such as clock towers, are also appropriate. Building materials, such as wood, stucco and brick, should reflect the building surfaces in the Junction area and be consistent with the village core concept.

- Traditional architecture elements, such as gabled, hip or shed roofs, are encouraged. These traditional styles should also be moderated with contemporary architectural styles. Solid glass walls should not be used.
- All mechanical equipment shall be screened from the public view and, where possible, integrated into the building as an architectural element.



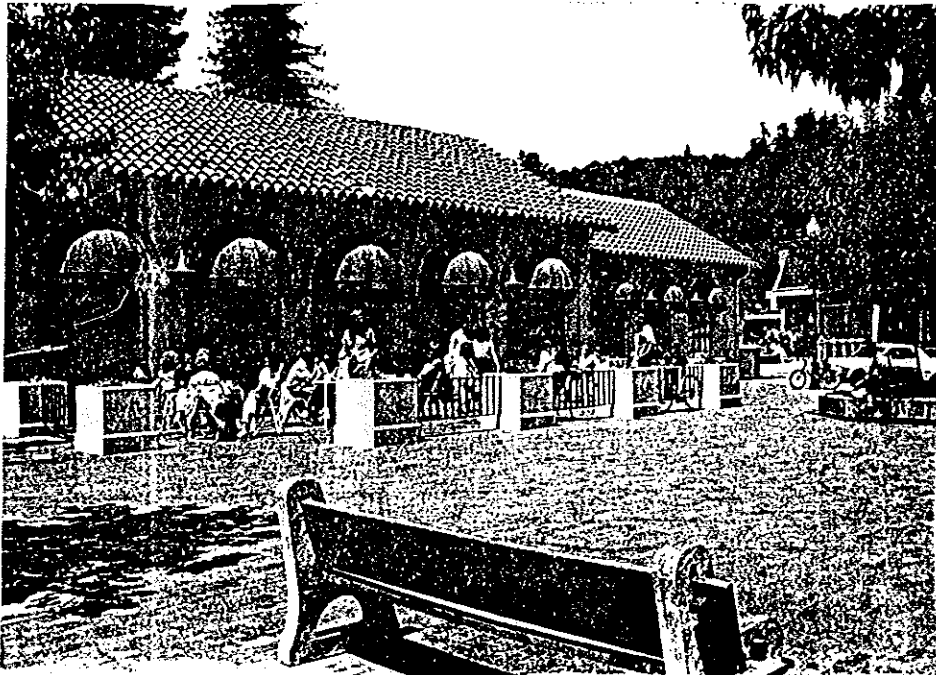
Two-story building in Downtown Mill Valley



Service area behind Cala Foods

SITE PLANNING & DEVELOPMENT GUIDELINES

- Rear service areas, including trash areas, shall be screened from public view.
- Courtyards, atriums and other plaza-like spaces that offer opportunities for outdoor gathering or eating are encouraged to improve the vitality of the pedestrian experience.
- The selective use of awnings is encouraged to add color and texture to the pedestrian areas. Awning colors should be subtle.
- Architectural materials should use natural colors, reflecting, where possible, the existing environment.
- All utilities located along the frontage area of any development shall be placed underground.



Litton Square in Mill Valley

LANDSCAPE GUIDELINES



LANDSCAPE GUIDELINES

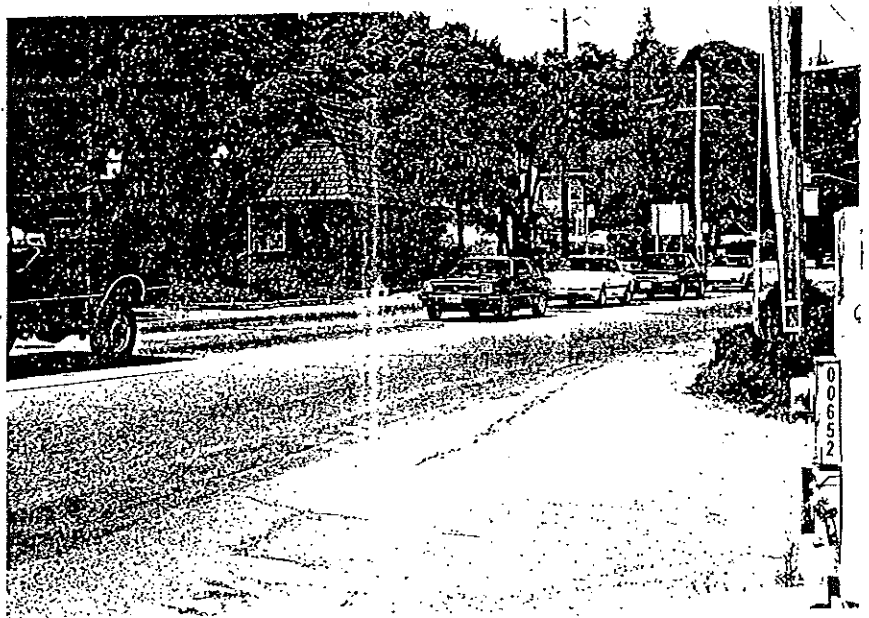
3.1 OVERALL LANDSCAPE CONCEPT

The Tam Junction area represents a transition zone and gateway from Highway 101 and the communities beyond to the residential areas of Tam Valley and Mill Valley and the western two-thirds of the County. This transition area will use the architecture of the existing and future development to reflect the concept of a village center surrounded by residential neighborhoods while using the landscape concept to reflect the transition from marsh and wetland open space to the heavily vegetated valley residential areas. The architecture will be neither rustic nor urban and should reflect a village image in scale and detail. The Junction lacks a cohesive landscape theme and presents an unfavorable visual image to both the residents of the Tam Valley and Mill Valley communities and visitors to and through the area.

The concept of the Junction as a village center in a landscape transition zone has several contributing elements. The linear elements of the circulation system, Shoreline Highway and Flamingo Road, which form the framework of the village core area. These linear elements can be strengthened by the formal, repetitive placement of street trees occasionally broken with opening into the developed areas. These open areas, which can include parking areas and plazas, can be further softened by the placement of massing of shrubs and ground covers. Plant materials common to Northern California particularly drought tolerant natives are the basic plant palette for the Junction. Appendix A reflects the list of plant materials preferred by the Marin Municipal Water District. Non-native plant materials can be blended with the preferred natives to reinforce the transition from marsh to urban to rural environments.

The Tam Junction landscape concept focuses on the public rights of way along Shoreline Highway and Flamingo Road as the main contributors to the visual character of the Junction. The linear aspects of the landscape is meant to minimize the dominance of the built environment, particularly during the period before the existing buildings are improved or replaced. The landscape concept also seeks to reduce the visibility of the parking areas from the roadways while creating an amenity of the open space network. The open space landscaping and the roadway landscaping is intended to extend into the commercial and residential areas around to parking lots and between buildings.

Almonte side of Shoreline Intersection



LANDSCAPE GUIDELINES

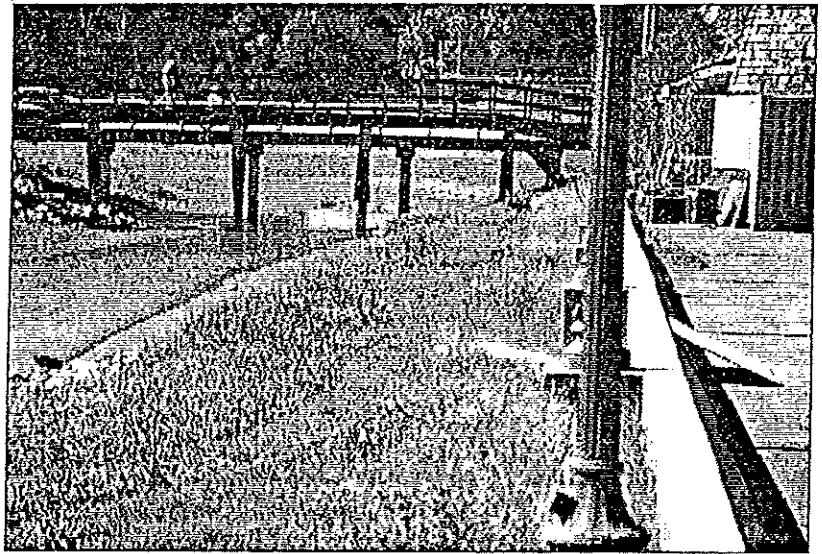
3.2 EDGES

3.2.1 Marsh/Wetland Edge

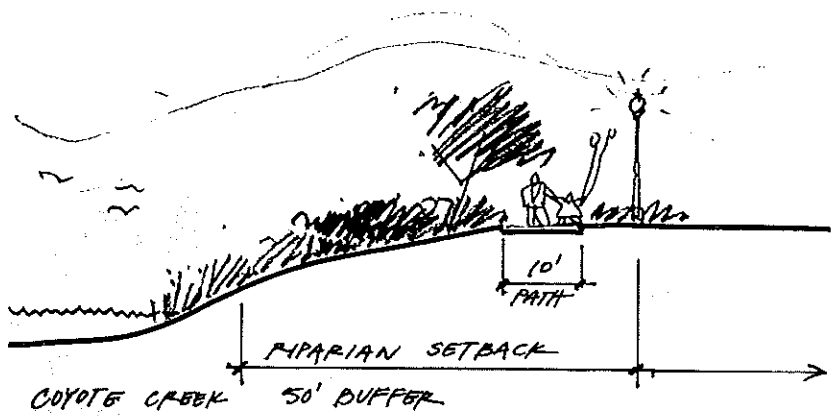
Wetland and riparian species are recommended along Coyote Creek and as part of the open space area adjacent to the existing marsh. The following sketches illustrate the marsh/wetland edge condition for the seam between the existing marsh area and the area now occupied by the miscellaneous small construction services on the Martin Brother's Triangle.

3.2.2 Open Space Edge

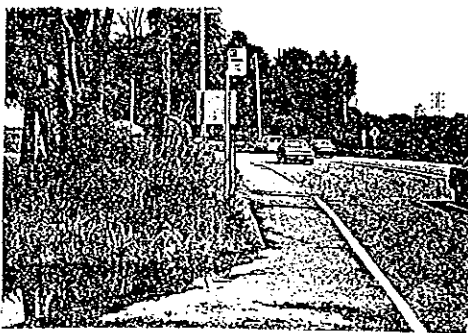
The Tam Junction conceptual plan recommends an eventual transition from the existing uses of vehicle and material storage now found on the Martin Brothers' Triangle to either passive or active open space. Of primary interest to the community is the relocation of the vehicles, equipment and storage facilities in the eastern portion of the property adjacent to the marsh. Once the existing clutter is relocated, the edge area should be left to reestablish itself as a riparian area under natural conditions. Depending on the eventual use of the remainder of the area, a variety of edge treatments could be applied. If the area is used for active recreation, such as field sports, a master plan will be required to locate the facilities, including a pathway system.



Coyote Creek Edge



Edge treatment along Coyote Creek



Existing conditions (marsh)
along Almonte Boulevard



Equipment and vehicle storage eastside of Shoreline

LANDSCAPE GUIDELINES

If the area is maintained as a passive open space area and allowed to return to a near natural state, a less detailed master plan would be required.

The Junction Plan recommends, that the area east of Shoreline Highway, including the strip of commercial, eventually be purchased and converted to open space. This action will be expensive and require a significant effort in terms of time and acquisition negotiation. An edge will be created between the open space area and the back side of the existing buildings. This edge should have a buffer area separating the open space from the buildings.

A landscaped buffer area, consisting primarily of riparian plant material, including trees and mounding shrubs, can be established along the line of buildings to screen the structures from the open space area and create a visual amenity.

Coyote Creek, on the west side of the bridge and where possible, on the east side of the bridge, shall have a landscaped buffer extending 50 feet from the top of the bank as determined by the County Planning Department. Riparian vegetation, native to the area, shall be planted or replanted in the buffer area. Consideration should be given to the maintenance requirements of the Flood Control District.

Public Access to the creek and the marsh areas shall be incorporated into any development proposed adjacent to the marsh or creek.

3.2.3 Residential

The Countywide Plan and the Tam Area Community Plan recommend including residential development in concentrated commercial areas to bring more daytime and nighttime life into the commercial areas and to reduce vehicle trips in the County. Incorporating residential development in the Junction can be accomplished if the underdeveloped

or undeveloped areas are included as part of a planned development.

3.3 STREETScape

3.3.1 Shoreline Highway and Flamingo Road

Within the Junction area, an area to be used as a landscaped setback for buildings along Shoreline shall be established as a condition of approval for a new development project. An alternative to conditions dependent on future development is to create an improvement district in the Junction with participation by the current property owners to establish a Landscaping and Lighting District to improve the gateway image.



Mixed use development in Mill Valley

Preservation of view corridors to marsh area

LANDSCAPE GUIDELINES

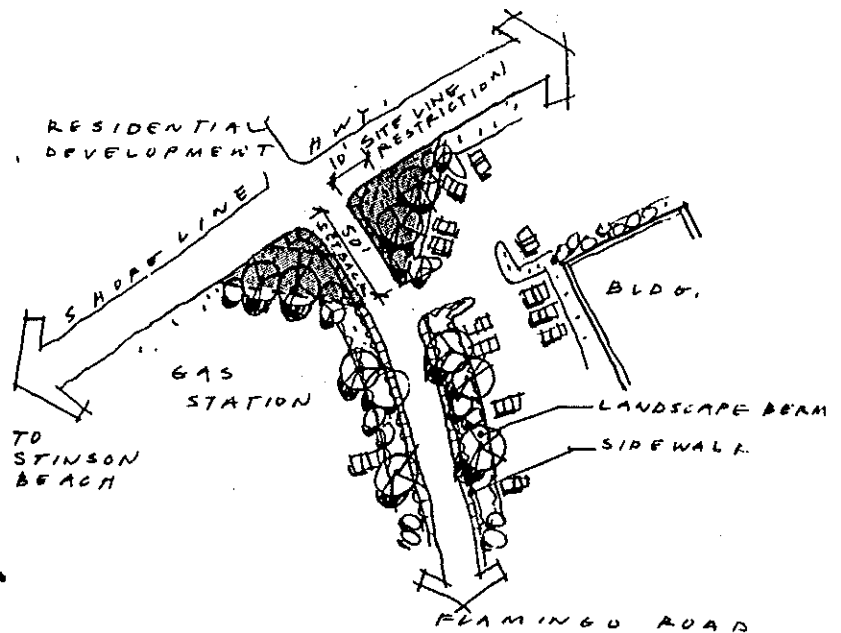
A landscaped setback area of fifteen (15) feet minimum from the right of way line/property line would be created for the installation of plant materials consistent with the list in Appendix A.

- Future development projects shall conform to the setback area and shall include in their landscape plan a program for satisfying the intent of the community to create an aesthetically pleasing gateway image.
- Along Shoreline Highway and Flamingo Road, the minimum distance of a landscaped area from the back face of the curb to the sidewalk is four (4) feet, except in those areas where bus stopping zones or mail deposit zones may be located.
- Street trees will be spaced in the landscaped area along Shoreline Highway to achieve the community's objective to create a gateway image. Spacing should not exceed twenty (20) feet and should be reflective of the specific requirements of individual tree species.
- Access to the buildings or parking areas along Shoreline or Flamingo Road will be designed as part of the development proposal to include an area where the landscaping is intensified so that an entry statement can be created. This area would also include project monument signs or directory signs. Sight line requirements, as stipulated in the Marin County Code shall govern placement of landscaping and entry area signs.
- Bicycle racks shall be incorporated into all new development. These racks shall be incorporated into the overall project design and be unobtrusive.

3.4 GATEWAYS

Gateway areas with distinctive landscape, lighting and signage elements should be created at the three major (Shoreline Highway at the Coyote Creek Bridge, Shoreline Highway at Almonte Boulevard and Shoreline Highway at Flamingo Road). One minor gateway (Flamingo Road at the entrance to the "Cala" area and the existing gas station) should be recognized. Design features, including landscaping, will identify the gateway areas as entry points to the village core area and establish the aesthetic quality of the Junction as a gateway to Mill Valley, Tam Valley and West Marin.

- Accent trees with spring flowering or fall accent colors should be incorporated into the gateway (see Appendix A for drought tolerant plant materials).



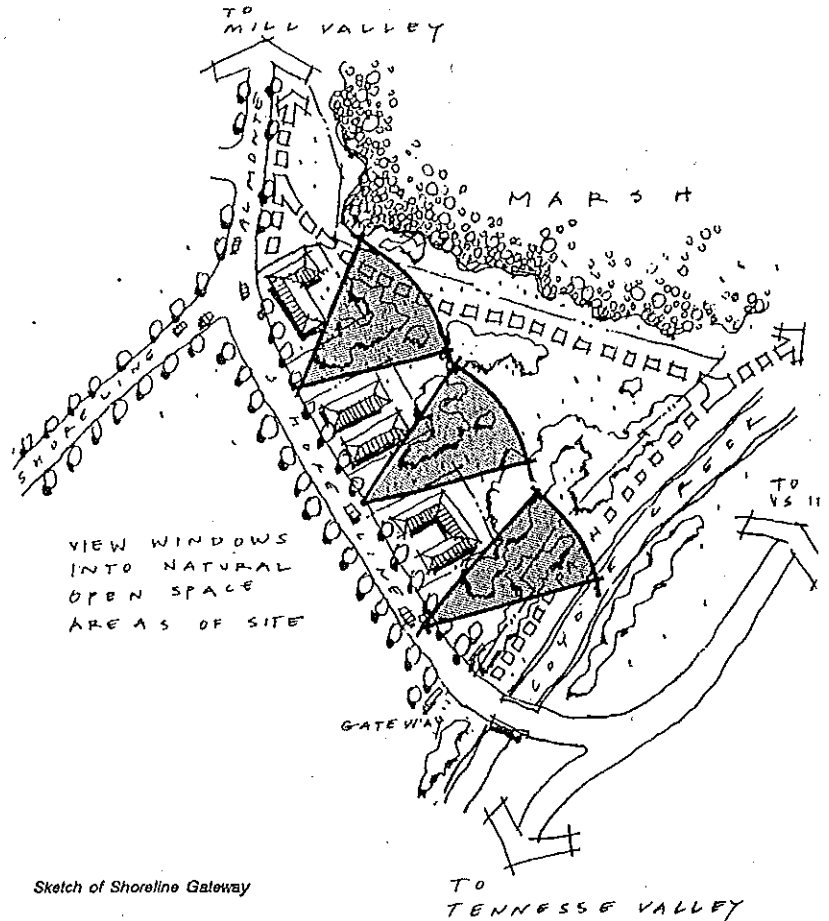
Setback at Shoreline and Flamingo

LANDSCAPE GUIDELINES

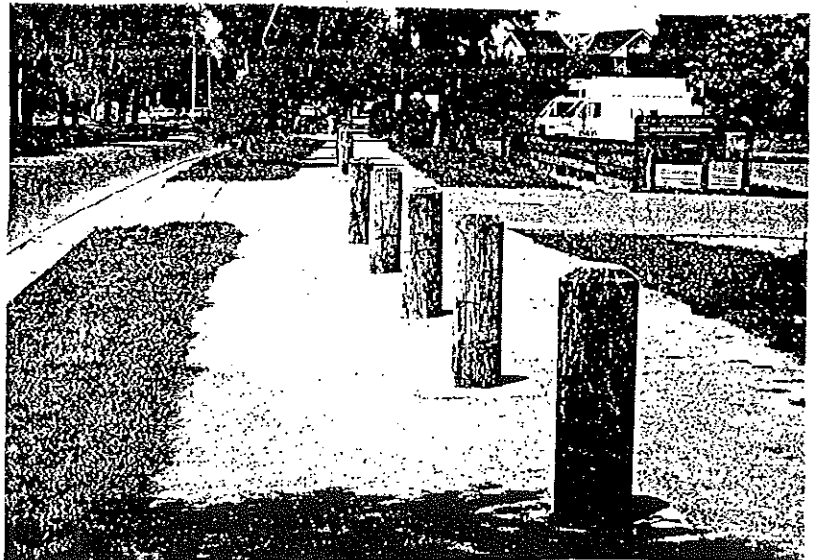
- Shrubs and ground covers producing seasonal colors should be incorporated into the gateway landscape (see Appendix A).
- Trees should be massed, and of sufficient size when mature, to create a gateway image. Consideration should be given to the relationship of the trees to be buildings of the village core and to the existing background of Tam Valley and the existing residential areas around the Junction.
- Pedestrian crosswalk areas should be identified by paving techniques that employ surface textures and markings other than the asphalt materials and white lines commonly used in the Junction area. Colored, textured, modular paving units and other similar techniques should be considered for use as crosswalk delineators.
- Bollards, or other edge identification elements, should be considered along the walkway edges to reinforce the gateway image and the circulation pattern.

3.5 OPEN SPACE

Open space is the key element of the Tam Junction Improvement Plan. The area east of shoreline Highway from the Coyote Creek Bridge to the Mill Valley City limits is considered to be the major area of open space in the Plan. Currently occupied by a varied collection of land uses and structures, this area could eventually be redeveloped for open space purposes providing active and passive recreation opportunities and becoming a visual amenity for the community. It is assumed that reclaiming the area, including the "Martin Brothers' Triangle", for open space will require a significant amount of time and money and may occur beyond the useful time frame established for Community Plan.



Sketch of Shoreline Gateway



Bollards used to define circulation patterns

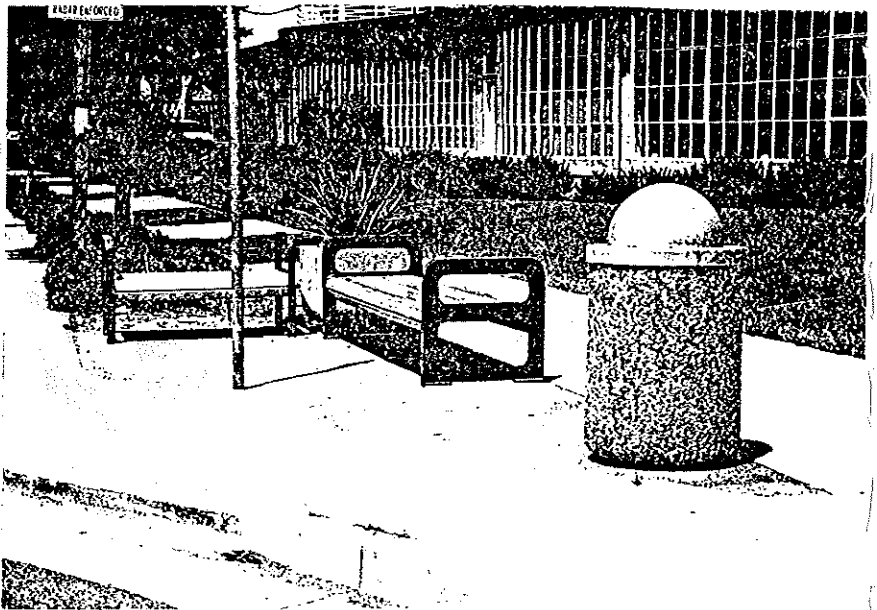
LANDSCAPE GUIDELINES

- The open space area is intended to be a combination of marsh edge, enhanced wetlands and native vegetation. More formal park and recreation areas are also viable design features. The preserved and enhanced as the primary landscape element around the perimeter of the open area, connecting it with the existing marsh.
- Bicycle and pedestrian pathways, as part of the network, should be incorporated into the open space element.
- The entire area, east of Shoreline Highway, should be planned as a single unit when proposals to change any of the existing uses are initiated.
- The Plan should include alternatives that reflect a mixture of uses including the primary objective of total open space on the east side of Shoreline.
- composition of the wheelstops shall be compatible with the overall design of the Junction area.
- Randomly positioned planting areas should be included in the design of parking areas to break up monotonous bays of parked vehicles. Large plant materials, including trees shall be located properly to avoid conflict with the opening of vehicle doors.
- Trees shall be installed in parking areas at the minimum rate of one tree of every five parking stalls.
- Street furniture shall incorporate the colors, textures and forms of the buildings in the Junction area as appropriate. Stone, rock, textured concrete, wood and metal are acceptable. Plastic and fiberglass products shall be avoided.
- Bollards placed in or along the public areas of Shoreline Highway and Flamingo Road shall be designed in accordance with the signage guidelines.

3.6 URBAN DESIGN FEATURES

Landscape elements in the village core area should be used to soften and enhance the visual impact of buildings, structures and parking areas. Special care shall be taken to screen service areas and other less attractive elements. Where feasible, landscape elements shall be used to frame views from the village core area to Mt. Tam, the marsh area and the hills around the valley.

- Landscaped areas shall be included in all parking areas. These planted areas shall be a minimum of six (6) feet from outside of curb to outside of curb (or edge to edge) to allow at least five (5) feet of landscaped area for ground covers, shrubs and/or trees. Lighting and signage (as appropriate) can be incorporated in these landscaped areas.
- To avoid damage to the landscaping by parked vehicles, wheelstops shall be included in all parking lot designs to prevent vehicular overhang. The



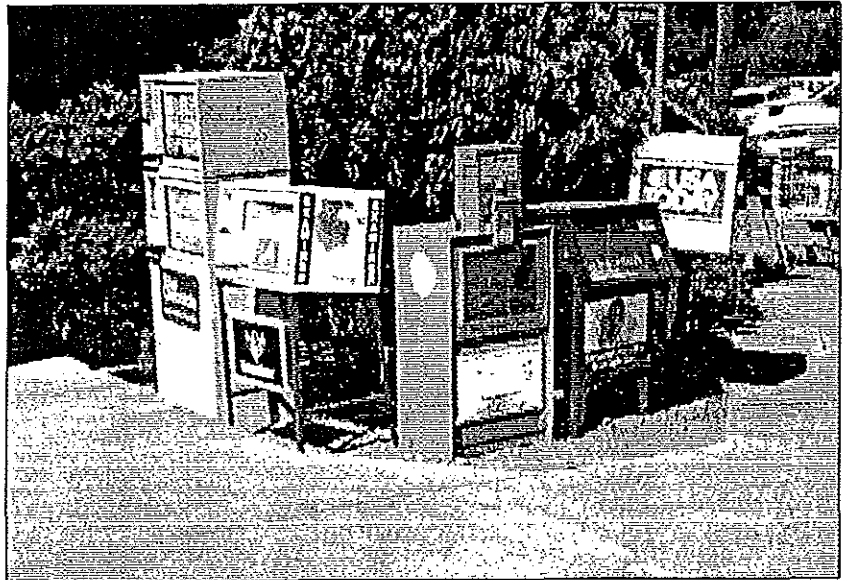
Urban Furniture

LANDSCAPE GUIDELINES

- Benches and seating areas shall be compatible with the adjacent buildings and contribute to the gateway image.
- Newspaper vending machines shall be limited to seven (7) in number per location (bus stop, store front, office entry). Each machine or combination of machines shall be consolidated and enclosed or screened or otherwise incorporated into the adjacent buildings to reduce the visual impact on the gateway area.
- Chain link fences are prohibited in the Junction area for all new projects. Existing chain link fences will be phased out as appropriate over time.
- Fences or walls used to screen trash container enclosures or transformer vaults shall be of architectural style and materials compatible with adjacent buildings or structures.
- No wall or fence should exceed six (6) feet in height. All walls shall be softened by landscape materials, either on or in front of the wall surface.



Bus stop along Shoreline Highway



Paper vending machine

SIGNAGE AND LIGHTING



SIGNAGE AND LIGHTING

Standards for signage and lighting are necessary to reinforce the concept of the Junction area as a core area for the Tam Area community. Without being inflexible, an area-wide set of standards is needed to regulate the appearance, size, location, materials, number and effective quality of signage and lighting to be used within the Junction area. More so than lighting, a coordinated signage program, with a defined hierarchy, can provide essential information in an efficient way to both inform and direct the members of the community and the general public in a safe and convenient manner. In addition to providing information and directions, good signage guidelines contribute to the image of the Junction area preferred by the community. An efficient yet aesthetically pleasing set of design guidelines and development standards minimizes confusion, promotes a desired image and contributes to the quality of life in community. Sign placement and sign design should be considered by the Design Review Board as an integral component of the architecture of each building or development proposal in the Junction.

These design guidelines and development standards are intended to reflect the existing county standards while allowing enough flexibility to encourage creativity. These guidelines to recognize the goal of the businesses in the Junction to be responsive to their markets by establishing identities that reflect personal tastes while contributing to the overall quality of the area. The creative use of color, light and materials can contribute greatly to the gateway image described in both the Community Plan and the Mill Valley General Plan.

Legal consequences for violation of the County's signage ordinances are included in Title 22.69 of the County Code.

These guidelines suggest that professional designers, manufacturers and skilled craftsmen be allowed to explore the use of traditional

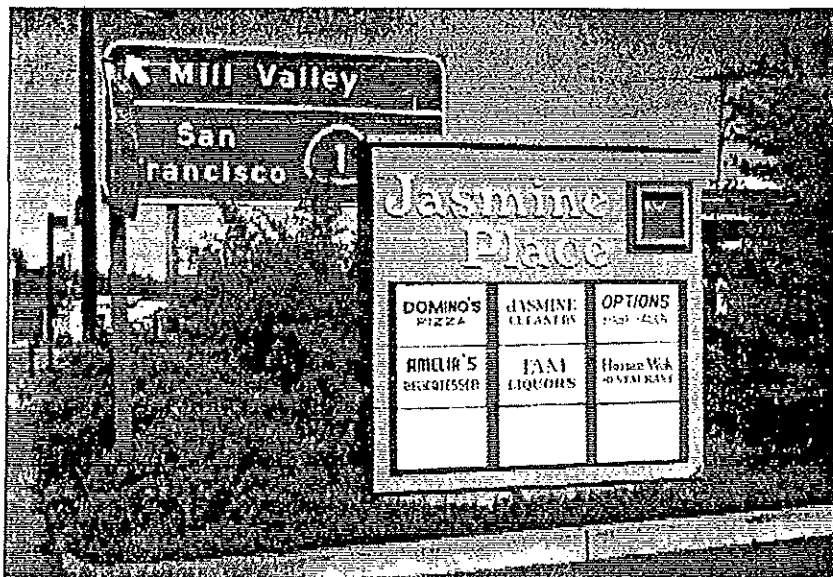
materials to create quality signs and lighting fixtures. It is the expressed goal of the community that the Junction area be improved and that good design is an important element toward that improvement. These guidelines have been crafted to meet the intention of the community's goals and are not meant to be an inflexible set of development standards. Signage submissions will be reviewed as an integral part of the architectural quality of each proposed development or redevelopment project and evaluated for its contribution to the overall visual and functional quality of the Junction. The Design Review Board and the County Planning Commission.

4.1 GENERAL SIGNAGE GUIDELINES

4.1.1 Review, Approval, Permits and Maintenance

All permanent signs erected or installed in the Tam Junction area shall be reviewed by the Design Review Board and shall conform to the sign regulations established by Marin County (see Title 22.69 of the Marin County Code). Submissions to

Collection of signs in Tam Junction



SIGNAGE AND LIGHTING

the Design Review Board shall include plans, elevations and site location syndicating dimensions, construction materials and colors or proposed signs. The submissions shall be prepared by professionals trained in design or sign making. All permanent signs shall be issued a permit from the County of Marin. All signs having an electrified component shall be installed by a licensed electrical contractor or by a sign company licensed to install or construct electrified signs.

Any conditions not specifically addressed in these guidelines shall be controlled by Title 22.69 regulations established by Marin County. Any signs that do not conform to the established regulations will require acceptance by the County Planning Department.

The following general guidelines apply to all signage conditions:

- Acceptable signage materials are wood, stone, brass, chrome, stainless steel, aluminum, porcelain enamel, neon, tile, brick, concrete and painted fabricated sheet metal. Sign bases shall be concrete, brick, stone or similar material.
- All signage shall be designed to withstand earthquakes and wind loads without the use of visible bracing. All signs shall have proper anchorage.
- All signage proposed for attachment to a parapet wall shall include an analysis of the parapet construction establishing the ability of the structure to accommodate the proposed load.
- All signs shall be maintained in good order, be visually attractive and be free of vandalism. All abandoned signs or belonging to tenants no longer occupying space in the Junction shall be removed in accordance with Title 22.69.105. No sign shall be allowed to operate or exist in a condition that would extend into the public right of way or endanger the public.



Signage



Signage

4.1.2 Commercial Signs

Signs announcing commercial activities in the Junction may be attached to the face of a building, canopies or awnings, but shall not be placed higher than building eaves, fascia or parapet line. Signage shall be limited to one half square feet of sign per lineal foot of store frontage. No single sign



Restaurant Sign

SIGNAGE AND LIGHTING

shall exceed 50 square feet. In certain cases, where major tenants require, exceptions may be considered through the Design Review process. Signage materials may be wood, fabricated metal, porcelain enameled steel, ceramic tile, or individual letters.

- Letters shall be constructed of individually fabricated metal or porcelain enameled panels and copy. Acceptable materials include wood, chrome, stainless steel, aluminum and painted channel letters fabricated or sheet metal.
- Window signage shall not exceed five per cent of the window area. Window signage colors shall be compatible with the exterior architectural detailing. Only the store name and/or identifying symbol, operating hours, emergency telephone numbers and security information are allowable copy. Temporary signs shall comply with the requirements contained in Section 4.1.6.

4.1.3 Office Signs

Office buildings may have a single tenant or multiple tenants. Single tenant buildings may have signage at or near the top of the building. Signage for multi-tenant buildings should be near the building entrance.

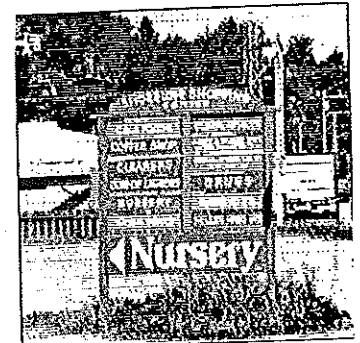
- Office signs may be attached to the building face below the building wall roof line. The sign area shall be limited to one-half square feet per lineal foot of frontage and in no case shall the total signage attached to any building face exceed 50 square feet. Signs shall be individual letters with no visible mounting hardware and not internally illuminated or backlit. No signs shall extend above the roof edges, as specified by Title 22.69.070.
- One freestanding tenant monument sign is allowed near each building entrance. These monument signs shall not be visible from Shoreline Highway or Flamingo Road. These



Clutter of window signs

signs shall not exceed five feet in height or 24 square feet in area per side, and must be compatible with construction materials and architectural detailing of the building.

- One sign may be mounted on each building adjacent to the entry door to identify the occupant, hours of operation and emergency notification numbers. This building mounted sign shall not exceed seven square feet in area and no copy will be located higher than six feet from the finish grade.



Directory Sign

- Acceptable materials include wood, chrome, stainless steel, aluminum or channel letters fabricated of painted sheet metal. Sign bases shall be of architectural concrete, brick, stone or similar material.

4.1.4 Residential Signs

Multi-family housing require curbside directory signs providing information for visitors and deliveries.

- Residential project monument signs should be placed at or near the Entrance to the residential cluster.

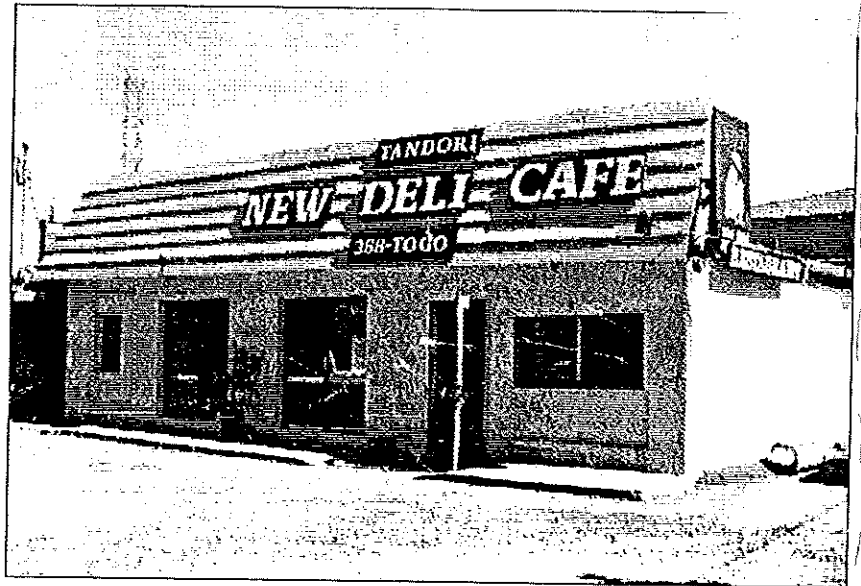
SIGNAGE AND LIGHTING

- Entry directory signs shall not exceed five square feet. Sign copy can include building addressing, regulatory information and a map or block model of the complex. The design and color of the signage package shall be compatible with the architecture of the residential development.

4.1.5 Restaurant Signs

- Restaurant signs on buildings can be used in place of driveway monument signs.
- Only one sign will be allowed at a driveway or parking area entry point. One sign will be allowed on the restaurant building. No more than two signs will be allowed for any restaurant in the Junction.
- The restaurant signage shall not exceed six feet in height, measured from finished grade, and a total of 18 square feet.
- Acceptable materials include sand blasted or painted wood, carved wood, ceramic tile, chrome, stainless steel, aluminum, painted channel letters fabricated of painted sheet metal. Bases for the restaurant signs shall be architectural concrete, brick, stone or similar material.

Junction. Materials, colors and type face of lettering shall be compatible with the architecture and nature of business of the existing use. Only one temporary sign per parcel is permitted at any one time. Temporary signs shall not advertise a product.



Restaurant Signs

4.1.6 Temporary Signs

Temporary signs shall only be used for construction, marketing, leasing, retail sales and special events in accordance with Title 22.69. All temporary signs will be subject to review and approval by the Design Review Board and the County Planning Commission. Temporary signs will be designed and produced by professional designers or sign-makers and will be of high quality, exhibit imagination and be constructed of materials appropriate to the expected life span of the sign and the general weather conditions. All temporary signs shall be compatible with the approved design character of other informational signs in the



Restaurant Sign



Sketch Design

SIGNAGE AND LIGHTING

- One construction or leasing sign is allowed for a development or redevelopment project. The maximum sign of any temporary sign is 32 square feet in area. No leasing sign may be attached to the building. Temporary sign may be of painted plywood or wood product composition and may contain any graphic information describing the project and the principal parties to be contacted. Temporary signs may be erected upon approval of a ministerial sign permit issued by the Marin County Planning Department. Temporary signs related to leasing shall be removed within 30 days after the occupancy, unless further extensions are permitted. No temporary signs shall be permitted for more than two (2) years.
- Special event signs, including but not limited to the Rhubarb Review, the Mountain Play and youth athletic league registration, may be erected no sooner than seven days preceding the special event, such as a special sale, and must be removed within three days following the event (Title 22.690.105).
- Temporary window mounted signs providing leasing information or announcing special events, such as special sales or services, shall be allowed, provided that no such sign (or group of signs when aggregated) exceeds 10 percent of the total window area or four (4) square feet of the window area in which they are displayed. All temporary window signs are subject to review by the Design Review Board.
- No windows may be painted or blocked to make them opaque.

4.1.7 Specialty Signs and Graphics

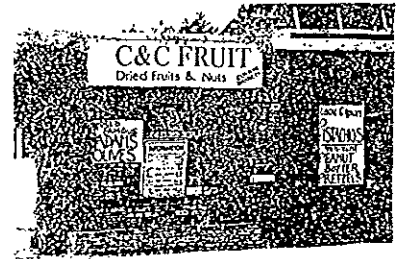
All specialty signs and graphics must be reviewed by the Design Review Board and approved by Marin County prior to fabrication.

- Marquees, with changeable copy announcing entertainment opportunities, shall be allowed, provided they are designed as an integral component of the architecture of the building and consistent with the overall color scheme and detailing of the building. Marquees shall not exceed 25 square feet in size.
- Kiosks providing information display space shall be allowed in the retail and residential areas or near bus stops provided they are compatible in style, scale, form, color and texture with the surrounding architecture. Kiosks located in plazas or courtyards in the retail and residential areas shall reflect the architecture of the adjacent buildings. Kiosks located in the public areas near bus stops shall be compatible with the overall village character of the Junction. Kiosks shall not exceed eight (8) feet in height to tip o highest point of structure.

4.1.8 Prohibited Signs

The following sign types are prohibited in the Tam Junction area.

1. Signs, such as those with any visible part that moves, shakes, revolves, rotates, flashes, blinks or has any noticeable mechanical motion of any description, or any other apparent visible movement achieved by electric, electronic or mechanical means. Electronic reader boards or fiber optic signs which may be proposed to provide specific information, such as traffic conditions on Shoreline High west of the Junction or parking conditions at the West Marin beaches or on Mt. Tamalpais, may be erected with the review by the Design Review Board and approval by the County Planning Department.
2. Signs with the optical illusion of movement by means of a design technique which presents a pattern capable of reversible



Temporary window signs

SIGNAGE AND LIGHTING

perspective, giving, the illusion of motion or changing of copy.

3. Signs, such as those with lights or illuminations which flash, move, rotate, scintillate, blink, flicker, vary in intensity and color or use intermittent electrical pulsation.
4. Lights arranged in strings.
5. Signs which incorporate projected images, emit any sound for the purpose of attracting attention or involve the use of live animals or human beings.
6. Signs which are parallel to a building, canopy or arcade, but project more than 18 inches from the building, canopy or arcade.
7. Signs which extend above the roofline or above the top of a front wall parapet of the building, whichever is higher.
8. Signs not permanently affixed or attached to the ground or any structure, except for temporary emergency barrier signs.
9. Signs which are:
 - a. Structurally unsafe.
 - b. Constitute a safety hazard due to inadequate maintenance or repair.
 - c. Capable of causing electrical shocks to persons likely to make incidental contact.
 - d. In any way obstructs the view of, may be confused with, or purports to be an official traffic sign, signal or device or any other type of official sign.
 - e. Uses any words, phrases, symbols or characters implying potential danger or the need for stopping or maneuvering of a vehicle or bicycle.

f. Creates an unsafe distraction for vehicle operators in anyway.

g. Obstructs the view of vehicle operators entering Shoreline Highway or Flamingo Road from any parking area, service drive, private driveway or other thorough fare.

h. Displays obscene, immoral or offensive material.

10. Off-premise advertising signs or any other sign not pertinent and clearly incidental to the permitted use of the property where located.

11. Signs which inhibit free access to a required door, window, fire escape or other required emergency exit.

12. Inflatable signs, including tethered balloon-like signs, including other moveable light objects designed primarily for the purposes of advertising.

13. A-frame or sandwich board signs.

14. Political signs.

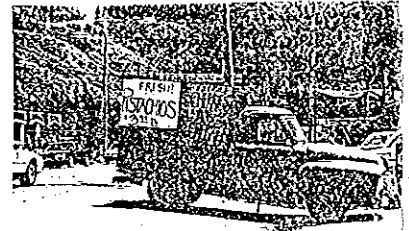
15. All new or renovated signs not reviewed by the Design Review Board and approved by the County Planning Commission.

4.2 GENERAL LIGHTING GUIDELINES

4.2.1 Review, Approval, Permits and Maintenance

The review, approval, permits and maintenance requirements that apply to signs erected in the Junction also apply to lighting.

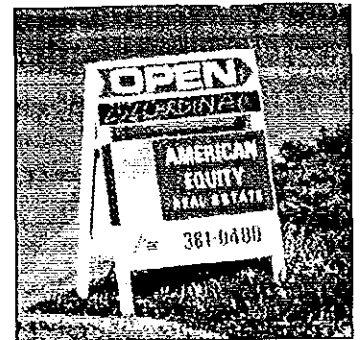
In general, site lighting shall be simple in design, in keeping with the village center concept. Lighting plans shall be a coordinated component of the architecture of any proposed development or redevelopment as well as being coordinated with any streetscape features along Shoreline



Prohibited signs



Merchant sign



A-frame sign

SIGNAGE AND LIGHTING

and Flamingo Road. Pedestrian scale light standards will be used near buildings with adjacent (front or side) parking lots. In general, warm lighting is preferred in the Junction area. Lighting will be carefully planned and placed to avoid affecting the adjacent residential areas, as well as those areas on the hillsides framing the Valley. Automatic timing devices, which maximize personal safety and conform to energy conservation principals, are required for all new redeveloped projects. All fixtures and free standard support standards shall conform to state and local safety and illumination standards and requirements. All lighting systems shall be installed by licensed electrical contractor.

4.2.2 Building Exterior Lighting

- Building exteriors shall be illuminated by uplighting only. Lighting which is used to wash walls in light, and lobby area lights which illuminate areas outside the lobby area are discouraged unless the lighting accentuates key architectural elements and contributes to the overall Village concept. Only warm lighting shall be used.
- Colored lighting on building exteriors or in landscaped areas is prohibited.
- Neither facade nor entire building exteriors shall be illuminated.
- Lighting plans that contribute the overall Village Center concept by enhancing the architecture or landscape are encouraged. All lighting plans shall consider the impact on the residential areas adjacent to Shoreline Highway, Flamingo Road and Cardinal Road. No intrusion into the residential areas, including those on the hillsides above the valley floor shall be permitted.
- Lighting fixtures should not be conspicuous during daylight hours.

- Lighting shall not be allowed to shine in the eyes of vehicle operators.
- Search lights are prohibited.

4.2.3 Pedestrian Area Lighting

- High pressure sodium or other energy efficient lighting may be attached to building walls, including carports and patio walls if residential units are developed in the Junction. Where necessary, particularly in large open areas that might be included in the retail area, post or bollard lights might be required for pedestrian safety.
- Low level, pedestrian scale lighting shall be provided along pedestrian walkway areas.
- Pedestrian area lighting should be used to enhance the landscaped areas and reinforce the architecture.

4.2.4 Parking Area Lighting

- Lighting fixtures for access drives, parking areas and internal circulation areas shall be mounted on poles or building walls at a height sufficient to provide an illumination ratio of 3:1 (average to minimum), maintaining an average of 1.0 foot candles, and should be hooded to avoid glare impacts on surrounding areas.
- Energy sufficient lighting (high pressure sodium fixtures) shall be used.
- Light bollards, or similar low level (less than 10 feet in height), small scale lighting fixtures should be used to illuminate pedestrian walkways in or leading to parking areas. The form, texture and color of pedestrian area lighting shall be compatible with the architectural character of the adjacent structures.

SIGNAGE AND LIGHTING

4.2.5 Service Area Lighting

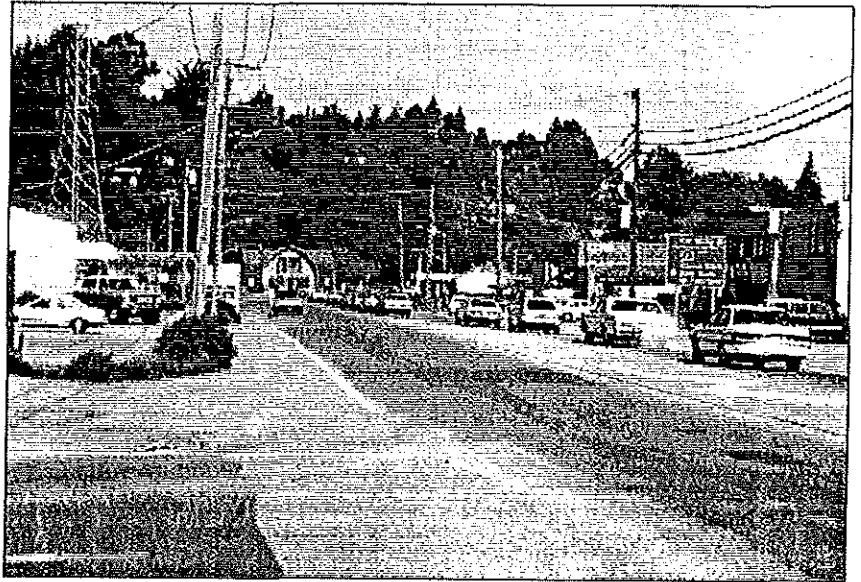
Service area lighting shall be contained within the service yard boundaries. Light spillover to adjacent areas should be minimized. Service area light sources shall not be visible from Shoreline Highway, Flamingo Road or the residential area along Cardinal Court.

4.3 UTILITIES

All overhead utility lines should be buried underground, particularly along Shoreline Highway from the Coyote Creek bridge to the intersection with Almonte Boulevard, concurrent with new construction. The County should coordinate with the Pacific Gas and Electric Company (P.G.&E. and utilize the existing programs to place utility lines underground.

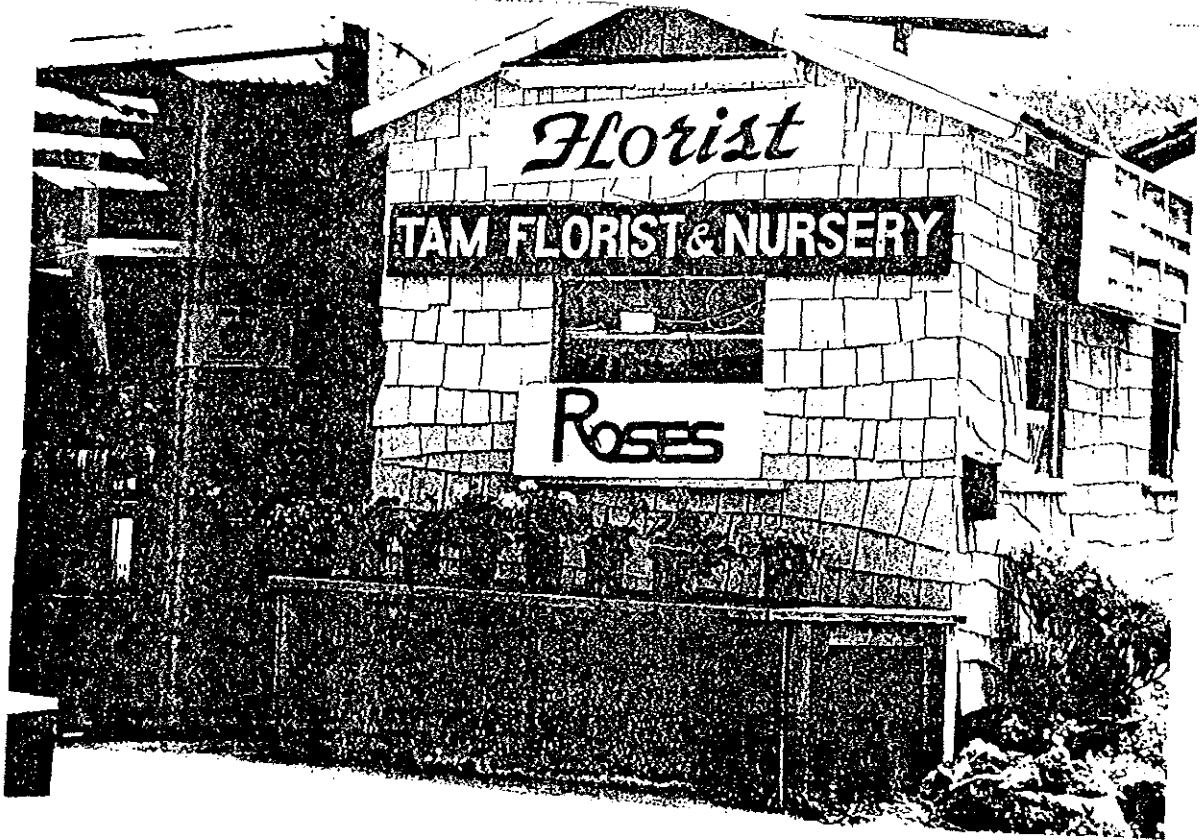
As development proposals are received by the County, the individual property owners and project sponsors will be required to participate in an undergrounding program.

Transformers and other utility structures should be installed in underground vaults. Where this is not possible, they should be screened from view from Shoreline Highway and the adjacent area using walls, screens or landscaping.



Overhead utilities along shoreline

APPENDIX A



LANDSCAPE MATERIALS

LANDSCAPE MATERIALS

The following list of plant materials is recommended by the Marin Municipal Water District:

<u>SCIENTIFIC NAME</u>	<u>COMMON NAME</u>
Trees	
<i>Albizia julibrissin</i>	Silk Tree
<i>Cedrus deodara</i>	Deodor Cedar
<i>Ceratonia siliqua</i>	Carob
<i>Eriobotrya deflexa</i>	Bronze Loquat
<i>Eriobotrya japonica</i>	Japanese Loquat]
<i>Eucalyptus sideroxylon rosea</i>	Red Ironbark
<i>Fraxinus oxycarpa</i> `Raywood'	Raywood Ash
<i>Fraxinus uhdei</i>	Shamel Ash
<i>Gleditsia triacanthos</i>	Honey Locust
<i>Grivillea robusta</i>	Silk Oak
<i>Lyonothamnus floribundus</i>	Catalina ironwood
<i>Maytenus boaria</i>	Mayten Tree
<i>Pinus canariensis</i>	Canary Island Pine
<i>Pinus halepensis</i>	Aleppo Pine
<i>Pistacia chinensis</i>	Chinese Pistache
<i>Platanus acerifolia</i>	London Plane Tree
<i>Prunus cerasifera atropurpurea</i>	Pissard Plum
<i>Pyrus kawakamii</i>	Evergreen Pear
<i>Rhus lancea</i>	African sumac
<i>Sapium sebiferum</i>	Chinese Tallow Tree
<i>Schinus molle</i>	California Pepper Tree
<i>Tristania conferta</i>	Brisbane Box
Shrubs	
<i>Abelia grandiflora</i>	Glossy Abella
<i>Agapanthus africanus</i>	Lily of the Nile
<i>Arbutus unedo</i>	Strawberry Tree
<i>Arctostaphylos desiflora</i> `HowardMcMinn'	Howard McMinn Manzanita
<i>Baccharis pilularis</i>	Prostate Coyote Brush
<i>Berberis thunbergii</i>	Japanese Barberry
<i>Ceanothus</i> `JulidPhelps'	Julia helps Ceanothus
<i>Centaurea cineraria</i>	Dusty Miller
<i>Cercis occidentalis</i>	Western Redbud
<i>Cistus purpureus</i>	Orchid-spot Rockrose
<i>Coleonema album</i>	Breath of Heaven

LANDSCAPE MATERIALS

SCIENTIFIC NAME

Dodonaea viscosa
Fremontodendron hybrids
Grevillea `Canberra'
Grevillea `Noellii'
Juniperus spp.
Mahonia aquifolium
Nandina domestica
Nerium oleander
Photinia serrulata
Pinus mugo mughus
Pittosporum eugenoidos
Pittosporum tobira `Wheeleri'
Prunus ilicifolia
Prunus lyonii
Rhamnus alaternus
Trichostema lanatum
Xylosma congestum

COMMON NAME

Hop Bush
 Flannel Brush
 Canberra Grevillea
 Noelli Grevillea
 Juniper
 Oregon Grape
 Heavenly Bamboo
 Oleander
 Chinese Photinia
 Dwarf Mugo Pine
 Tarata
 Wheeler's Dwarf Tobira
 Holly-Leaf Cherry
 Catalina Cherry
 Italian Buckhorn
 Woolly Blue Curls
 Shiny Xylosma

Ground Covers

Arctostaphylos uva-ursi `PoinReyes'
Arctotis hybrids
Baccharis pilularis `TwinPeaks'
Ceanothus gloriosus
Coprosma kirkii
Coloneaster `Lowfast'
Gazania Species
Hypericum calycinum
Iberis sempervirens
Juniperous conferta
Phyla nodiflora (Lippia repens)
Rosemarinus officinalis cultivars
Verbina Species
Vinca minor

Point Reyes Bearberry
 African Daisy
 Twin Peaks Coyote Brush
 Point Reyes Ceanothus
 Creeping Coprosma
 Lowfast Coloneaster
 Gazania
 Saint-John's-wort
 Evergreen Candytuft
 Shore Juniper
 Lippia
 Rosemary
 Verbena
 Dwarf Periwinkle

Vines

Bougainvillea Species
Jasminum polyanthums
Parthenocissus tricuspidata
Wisteria Species

Bougainvillea
 Pink Jasmine
 Boston Ivy
 Wisteria

LANDSCAPE MATERIALS

SCIENTIFIC NAME

COMMON NAME

Palms

Phoenix canariensis

Canary Island Date Palm

Yucca gloriosa

Spanish Dagger

Perennials

Armeria maritima

Sea* Pink

Calendula officinalis

Calendula

Diets vegeta

Butterfly Iris

Iris douglasiana

Douglas Iris

Lantana montevidensis

Trailing Lantana

Lobularia maritima

Sweet Alyssum

Narcissus Species

Daffodil

Ranunculus

Buttercup

Thymus vulgaris

Common Thyme

CALIFORNIA NATIVES*

Trees

Aesculus californica

California Buckeye

Pinus sabiniana

Digger Pine

Populus fremontii Nevada

Nevada Male Cottonwood

Quercus agrifolia

Coast Live Oak

Quercus lobata

Valley Oak

Sequoia sempervirens

Coast Redwood

Shrubs

Arctostaphylos bakeri `LouiEdmunds'

Louis Edmunds Manzanita

Arctostaphylos densiflora `HowardMcMinn'

McMinn Manzanita

Ceanothos `Concha'

Concha Ceanothus

Ceanothos `FrostBlue'

Frosty Blue Ceanothus

Ceanothos thyrsiflorus `SnowFlurry'

Snow Flurry ceanothus

Ceris occidentalis

Western Redbud

Eriogonum arborescens

Santa Cruz Island Buckwheat

Eriogonum crocatum

Saffron Buckwheat

Eriogonum giganteum

St. Catherine's Lace

Eriogonum umbellatum polyanthum

Sulfur Buckwheat

Fremontodendron hybrids

Flannel Bush

* SOURCE: *Water-Conserving Plants and Landscapes for the Bay Area, East Bay Municipal Utility District, 1986*

LANDSCAPE MATERIALS

SCIENTIFIC NAME

COMMON NAME

<i>Garrya elliptica</i> Coast	Silktassel
<i>Heteromeles arbutifolia</i>	Toyon
<i>Mahonia pinnata</i>	California Holly Grape
<i>Myrica californica</i>	Pacifica Wax Myrtle
<i>Prunus ilicifolia</i>	Holly-Leaf Cherry
<i>Prunus lyonii</i>	Catalina Cherry
<i>Phamnus californica</i>	California Coffeeberry
<i>Phus integrifolia</i>	Lemonade Berry
<i>Ribes sanguineum</i>	Flowering Currant
<i>Ribes speciosum</i>	Fuchsia Flowered Gooseberry
<i>Ribes viburnifolium</i>	Evergreen Currant

Ground Covers

<i>Arctostaphylos</i> `EmeraldCarpet'	Emerald Carpet Manzanita
<i>Arctostaphylos hookeri</i>	Monterey Manzanita
<i>Arctostaphylos</i> `PoinReyes'	Point Reyes Manzanita
<i>Baccharis pilularis</i> `TwitPeaks', `PigeonPoint'	Dwarf Coyote Bush
<i>Ceanothus gloriosus porrectus</i>	Mount Vision Ceanothus
<i>Ceanothus hearstiorum</i>	Heast Ceanothus
<i>Ceanothus</i> `JoyceCoulter'	Joyce Coulter Ceanothus
<i>Ceanothus rigidus</i> `Snowball'	Snowball Ceanothus

Palms

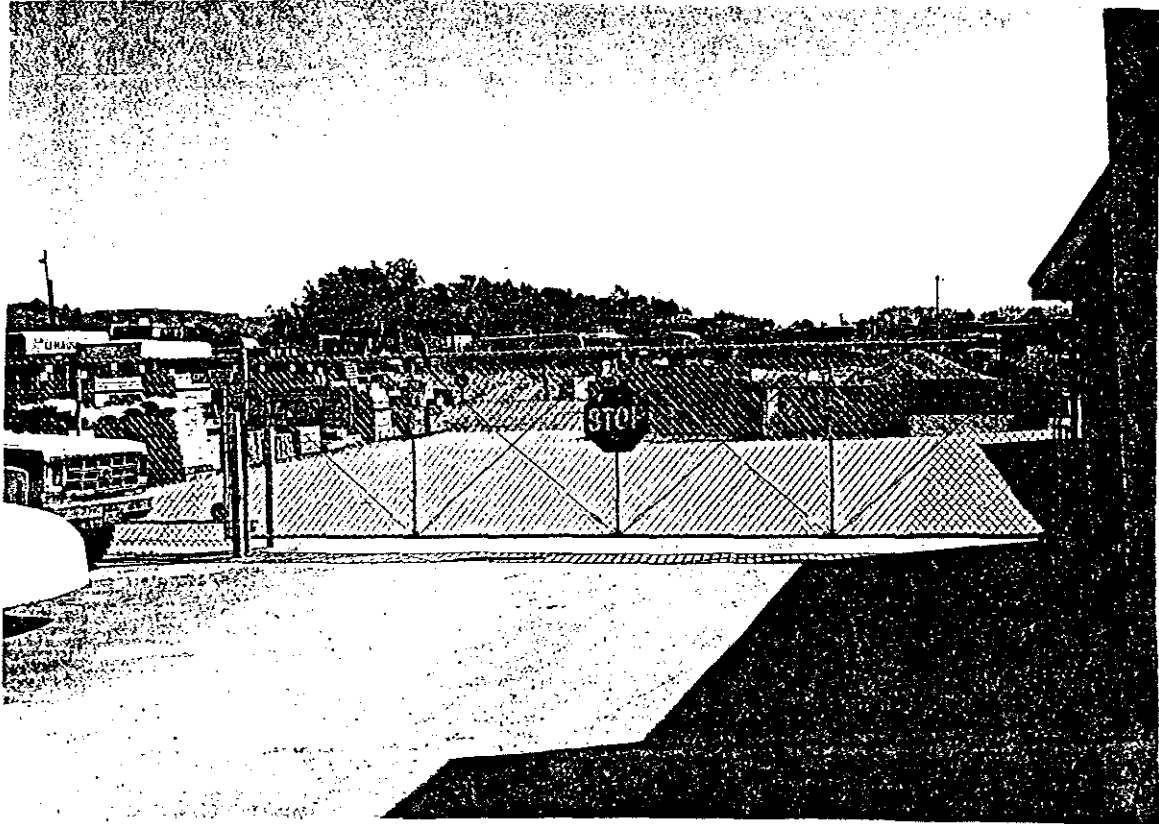
<i>Washington filifera</i>	California Fan Palm
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Herbaceous Plants

<i>Diplacus hybrids</i>	Monkey Flower
<i>Eschscholzia californica</i>	California Poppy
<i>Penstemon heterophyllus purdyi</i>	Beard Tongue
<i>Romneya coulteri</i>	Matilija Poppy
<i>Salvia clevelandii</i>	Cleveland Sage
<i>Sedum spathulifolium</i>	Sedum
<i>Zauschneria californica</i>	California Fuchsia

These lists represent plant materials that can be used in the Junction area to meet the goals and objectives of the Community Plan and the Tam Junction Plan. Other non-native and non-drought tolerant plant materials can be included in the landscape plans for projects in the Junction area but the project proponents must be able to justify their use to the Design Review Board.

APPENDIX B



RELATED COUNTY POLICIES AND REGULATIONS

RELATED COUNTY POLICIES AND REGULATIONS

The following list of policies and comments indicates how the Tam Junction Improvement Plan conforms to the existing policies and regulations included in the Marin County Wide Plan.

ENVIRONMENTAL QUALITY

Policy A-1 The Countywide Plan designates for permanent preservation open space in the following categories:

Resource Production: Agricultural, timber, fishing areas, sand and gravel deposits.

Resource Preservation: Water edges, watersheds, tidal areas, wildlife reserves, marshes, mudflats.

Scenic: Greenbelts, separators, open land, forests, grasslands, views trails.

Safety: Geologic risk areas, floodplains, noise areas, dikes, fire hazard areas.

Recreation: Public parks, trails, water sports areas, commercial recreation (golf courses, motels, stables).

Comments: The Tam Junction Improvement Plan recognizes the County's desire to preserve open space and natural resources along the marshes and wetland areas. The Junction Plan recommends that all (if necessary in phases increments), or part of the area on the east side of Shoreline Highway, between the Coyote Creek bridge and Almonte Boulevard be reclaimed as Resource Preservation open space.

Policy A-2

In the City-Centered Corridor the Marin County Open space District has established the following criteria for priority selections.

Criteria:

- Projects must be identified in the Open Space Element of the Marin Countywide Plan or other adopted general or specific plan.
- Projects should have the general support of the local jurisdictions within their sphere of influence.
- Acquisition of lands should be undertaken only for projects of district-wide significance.

Priority Selection:

Ridge tops will be relatively more important than stream beds and shorelines because they play a much more obvious role in shaping the development of the County.

Areas under immediate threat of development or irreversible damage will be given high priority.

The visual or ecological importance of the area in the County will have significance.

Local contributions in cash, land areas, or land use regulations will be considered but are not essential.

Projects which are contiguous and which will extend their usefulness to existing open space preserved areas are important.

Attractive purchase considerations and conditions will be significant.

RELATED COUNTY POLICIES AND REGULATIONS

Adoption of the Open Space Element of the Countywide Plan by the jurisdiction involved will be significant, but not mandatory.

Comments: The Junction Plan identifies the area east of Shoreline Highway as an area to be considered for open space. This area, which borders the existing marsh area and is significant for its wetlands potential, should be considered for acquisition because it meets the criteria for priority selection as specified by Policy A-2.

Policy A-8 Streams, estuaries, marshes, bays, and tidelands shall be maintained in their natural state.

Comments: These design guidelines recognize the importance of Coyote Creek as part of the riparian system in the Valley and specifies a 50 foot Stream Conservation Area.

Policy A-10 A diversity and abundance of wildlife and marine life shall be maintained. Vegetation and animal habitats shall be preserved wherever possible.

Comments: The Junction Plan and the design guidelines, in the discussion of Coyote Creek and the marsh area as riparian and open space elements, conforms to this policy.

Policy A-12 Adequate parks, recreation facilities, and open space shall be provided. Appropriate public access shall be established.

Comments: Public access to Coyote Creek and the marsh area are recommendations included in the Junction Plan and these design guidelines.

Policy A-16 Visual qualities and view potential of both natural and man-made settings shall be an equivalent consideration with other factors in any project or operation review. Tree-cutting and damage shall be avoided wherever possible.

Comments: Trees are not an issue in the Junction area but the visual amenities and view preservation and enhance guidelines conform to this policy.

STREAM CONSERVATION AREA POLICIES

General Policies

Policy B-1.1 Riparian systems, streams and their riparian and woodland habitat are irreplaceable, and should be officially recognized and protected as essential environmental resources, because of their values for erosion control, water quality, fisheries production, aesthetics and recreation.

Comments: The Junction Plan and these design guidelines conform to Policy B-1.1 by identifying Coyote Creek and the marsh area as sensitive elements of the riparian system.

Policy B-1.2 All perennial and intermittent streams, which are defined as natural watercourse shown as solid or dashed blue lines on the most recent appropriate USGS quad sheet, should be subject to these stream and creekside protection policies. A perennial stream is further defined as a watercourse that flows throughout the year (except for infrequent or extended periods of drought), although surface water flow may be temporarily discontinuous in some re on the most recent appropriate USGS quad sheet, should be subject to these stream and creekside protection policies. A perennial stream is further defined as a watercourse that flows throughout the year (except for infrequent or extended periods of drought), although surface water flow may be

RELATED COUNTY POLICIES AND REGULATIONS

temporarily discontinuous in some re and immediately after periods of precipitation, should be subject to these policies if it supports riparian vegetation for a length of 100 feet or more.

Comments: While this policy is more of a definition than anything else, these guidelines identify Coyote Creek as a perennial stream subject to the Stream Conservation Area (SCA) policies.

Policy B-1.3

A Stream Conservation Area (SCA) should be designated along all such streams, to consist of the watercourse itself and surrounding banks on both sides up to the high water mark and a strip of land extending laterally outward from the top of both banks, to a width of 100 feet on each side in the Coastal Recreation and Inland Rural Corridors and to a width of 50 feet on each side in the City-Centered Corridor. Where large tracts of land in the City-Centered Corridor are proposed for development, the 100-foot buffer should be applied, where consistent with legal requirements, and other planning and environmental goals. In the Coastal Recreation and Inland Rural Corridors, the zone should be extended if necessary to include an area 50 feet landward from the edge of riparian vegetation.

Comments: These guidelines are in conformance with Policy B-1.3. The guidelines on page I-25 specifies a 50 foot buffer area from the top of the bank along Coyote Creek. When redeveloped, the existing structures and parking areas east of the bridge shall conform to the setback.

Policy B-1.4

The following uses are permitted in the SCA by development permits, provided these uses are allowed by the underlying zoning: all currently existing structures and uses including reconstruction and repairs, necessary water supply projects; flood control projects; developments to improve fish and wildlife habitat; grazing of livestock and other agricultural uses; maintenance of water channels for erosion control and other purposes; road and utility line crossings; water monitoring installations; trails.

Comments The guidelines for the 50 foot buffer zone and edge treatment along Coyote Creek which includes road crossings and trails conform to this policy.

Policy B-1.5:

The following new uses are prohibited in the SCA: roads and utility lines, except at crossings; confinement of livestock, dumping or disposal of refuse, use of motorized recreational vehicles and any structural improvement (excluding repairs) other than those identified in Policy B-1.4, including residences, barns, and storage buildings, unless allowed by a development permit in Policy B-1.6.

Comments: The guidelines for the Junction Improvement Plan specifically excludes storage buildings. An existing maintenance road parallels Coyote Creek southwest of the bridge.

Policy B-1.6

Other uses may be allowed in the SCA by development permits, provided these uses are allowed in the underlying zoning, on existing parcels that fall entirely within the zone or on existing parcels where it can be conclusively demonstrated that development on any other part of the parcel would have a more adverse effect on water quality or other environmental impacts. Such development should conform to all policies for SCA's.

Comments: Only two parcels might be effected by this policy, the parcel currently occupied by the Dipsea Cafe and parking area and the undeveloped parcel behind the Video Droid and the pumping facility.

RELATED COUNTY POLICIES AND REGULATIONS

Preservation of Existing and Native Vegetation Policies

- Policy B-2.1** The retention of the natural vegetation on a SCA should be encouraged in order to realize many benefits, such as soil erosion prevention, stream, shade, etc. When vegetation must be removed and soil disturbed within the SCA, the area should be reseeded or replanted with native plants of the habitat as soon as possible removing broom and other aggressive exotic plants, so as to restore the vegetative cover.
- Comments: The guidelines on page I-25 conforms to this policy, recommending restoration of the riparian area along Coyote Creek southwest of the bridge.
- Policy B-2.3** Trees and shrubs to be planted along watercourses should include a variety of species that would naturally grow in or near the creek. Generally, exotic trees should be avoided.
- Comments: The guidelines on page I-25 conforms to this policy.
- Policy B-5.3** Damaged portions of SCA's should wherever possible be restored to their natural state. Portions of the channels that have been significantly altered for flood control have potential for urban open space uses as landscaped areas and paths.
- Comments: The guidelines on page I-25 conforms to this policy.

BAYFRONT CONSERVATION ZONE

Habitat Protection and Restoration Policies

- Policy C-1.1** The County shall preserve and enhance the diversity of wildlife and aquatic habitats found in the Marin County bayfront lands, including tidal marshes, seasonal marshes, lagoons, natural wetlands, and low-lying grasslands overlaying historical marshlands.
- Comments: The recommendation by the Junction Improvement Plan to create an open space on the east side of Shoreline Highway conforms to this policy.
- Policy C-1.2** Development should not encroach into sensitive wildlife habitats, limit normal range areas, create barriers which cut off access to food, water, or shelter, or cause damage to fisheries or fish habitats. Buffer zones between development and identified or potential wetland areas should be provided. Access to environmentally sensitive marshland and adjacent habitat should be restricted, especially during spawning and nesting seasons.
- Comments: If and when the various parcels east of Shoreline Highway are redeveloped or the uses change, development proposals will be required to address the buffer and access issues as part of an overall plan.
- Policy C-1.4** The County shall, through its land use and development regulations, foster the enhance of the wildlife and aquatic diking, filling, or dredging and/or are less protective of habitat value may be permitted when it can be proven that the resulting public benefit exceeds environmental costs and liabilities. Public benefits to be provided in the diked portions of the Bayfront Conservation Zone shall include but not be limited to: public access and recreational opportunities, educational or scientific opportunities, provision of housing (particularly housing developments which include low and moderate income housing), provision of essential water conveyance, transportation or utility services, and protection from flood or other natural hazards. On parcels greater than one-half acre in size, mitigation and/or compensation for habitat value lost due to diking, filling, or dredging shall be required, the amount to be determined by the County in conjunction with federal and state agencies.

RELATED COUNTY POLICIES AND REGULATIONS

Comments: This policy applies to the parcels east of Shoreline Highway between the bridge and Helen Avenue along Almonte Boulevard to the west, and all the waterside parcels to the Mill Valley boundary on the east.

Policy C-1.5 Freshwater habitats in the bayfront areas associated with freshwater streams and small former marshes should be preserved and/or expanded such that the circulation, distribution and flow of the fresh water supply is facilitated.

Comments: The guidelines on page I-25 which specifies the restoration of the riparian area along Coyote Creek on the east and west sides of the bridge, conforms to this policy.

Policy C-1.6 The County shall promote the retention and formation of large tracts of land within historic marshland areas and contiguous grassland areas as possible land banks for the protection of wetlands habitats.

Comments: The recommendations for the creation of an open space area east of Shoreline Highway is consistent with this policy.

Policy C-1.8 The County shall allow the transfer of the development potential of diked historic marshlands which are restored to total status or enhanced as wetlands habitat to upland sites, provided that development on the upland site complies with development standards for the protection of adjacent habitat areas.

Comments: Objectives, policies and programs indicate these areas will be protected (see Community Plan, LU.22).

Policy C-1.9 The County shall review all proposed development with the Bayfront Conservation Zone in accordance with the planned district review procedure in order to ensure maximum possible habitat protection. An assessment of existing environmental conditions (biologic, geologic, hazard, and aesthetic) shall be required prior to submittal of development plans.

Comments: The Tamalpais Area Community Plan, indicates that many parcels will be rezoned to a planned district. This designation conforms to Policy C-1.9, as it requires planned district review procedures.

Policy C-1.10 The County shall facilitate consultation and coordination with the trustee agencies (Department of Fish and Game, U.S. Fish and Wildlife Service, the Corps of Engineers, and BCDC) during environmental review and during review of other proposals for lands within the Bayfront Conservation Zone.

Comments: This policy is consistent with Policy C-1.9 above. The proposed zoning (RMPC) conforms to this policy. It is the County's responsibility to coordinate project review with the trustee agencies.

Protection of Environmental Quality Policies

Policy C-2.1 The County shall ensure that development in the County occurs in a manner which minimizes the impact of earth disturbance, erosion, and water pollution within the Bayfront Conservation Zone.

Comments: The guidelines of the Community Plan address this issue. It is the County's responsibility, as a condition of project approval, to ensure the minimization of adverse environmental impacts.

RELATED COUNTY POLICIES AND REGULATIONS

Policy C-2.3 The development and siting of industrial (and any other) facilities adjacent to bayfront areas should be planned to eliminate significant adverse environmental impacts on the water quality of the bay and marshes.

Comments: The RMPC zoning designation for the commercial area conforms with this County Policy to allow for uses to mitigate adverse impacts on the shoreline aesthetics and its water quality.

Protection from Geologic, Flooding and Other Hazards Policies

Policy C-4.1 Any development proposed for lands within the Bayfront Conservation Zone must be consistent with policies and proposals of the County Seismic Safety Element, including avoidance of areas that pose hazards such as differential settlement, slope instability, liquefaction, ground shaking and rupture, tsunami, and other ground failures.

Comments: The policies in the Community Plan LU.16 and LU.17 are in conformance with Policy C-4.1. The RMPC zoning designation is also in conformance with this policy.

Policy C-4.5 The County's regulatory procedures should reflect 100-year floodplain areas.

Comments: The guidelines for the Junction area specifically reference a finished floor elevation of 8' above mean sea level (MSL) as a requirement according to County standards.

Public Access and Bayfront Recreation Policies

Policy C-5.1 Public use of the shoreline areas is desirable and should be encouraged consistent with ecological and safety considerations.

Comments: Any plan for the redevelopment or change of use in the area east of Shoreline Highway should include public access to the marsh area as part of a path system or marsh habitat viewing area, consistent with ecological and safety considerations.

Policy C-5.2 The County shall ensure that public access is provided and protected along the bayfront and significant waterways. The County views public access easements, gained through offers of dedication, as a condition of development plan approval, as the primary means available to increase public access opportunities.

Comments: The overriding purpose of the RMPC zoning designation is to require development plans from project applicants that will include provisions for views, public access and dedicated easements.

Policy C-5.3 The County will accept, as resources permit, public access easements where the offered easement is in a developed area (density of one unit per acre or greater) and substantial use could be expected by local residents. Where the County accepts an easement, it will be responsible for signing, providing appropriate facilities, and maintaining the easement. If the County does not accept an easement, it shall attempt to find appropriate public or private agencies to do so.

Comments: This policy shall be a condition of approval for projects proposed in the Junction area. This policy is not specific about access to the bayfront or waterways and could be applied to easements through new developments that might be proposed on the west side of Shoreline. An example of such an easement might be a pedestrian pathway that connects the commercial development along Shoreline Highway with the residential areas adjacent to Flamingo Road. This easement could be used to safely

RELATED COUNTY POLICIES AND REGULATIONS

separate pedestrians from vehicles in the area commonly referred to as the "Cala Freeway" should a development proposal reconfigure that area in the future.

Policy C-5.4

The County shall evaluate potential new public access areas in order to determine the feasibility of providing access and the priorities for acquisition, based on the following criteria: desirability of the site, capacity to sustain use without significant adverse impacts on the bayfront habitat and wildlife, potential for hazard to public safety or health, availability of other public access points in the area, and compatibility with adjacent land uses.

Comments: The Junction Plan is in conformance with this policy since it is the County's responsibility to implement this policy as part of its review process.

Policy C-5.5

Public access should be sited and designed to facilitate public use and enjoyment of the bayfront lands. Public areas should be clearly marked, and continuous ten-foot walkways from the nearest roads to the shoreline and along the shoreline should be provided. Public access areas should be designed to minimize possible conflicts between public and private uses on the properties. Walkways should generally be set back at least ten feet from any proposed structure.

Comments: The guidelines on page illustrates the walkway recommended as a part of the circulation element providing access to the marsh area east of the Shoreline Highway.

Policy C-5.6

Within the Bayfront Construction Zone, provision should be made for recreational development and access to the shoreline marshes for such uses as fishing, boating, hunting, picnicking, hiking, and nature study. There should be provisions for both separated wildlife preserve and more intensively used recreational uses along the bayfront. Every available appropriate means of providing public education regarding the value of shoreline preservation and the shoreline as an educational laboratory shall be encouraged.

Comments: It is the intent of the Junction Plan, by using the RMPC zoning designation, to encourage planning for the area east of the Shoreline to include a pedestrian circulation system near the marsh area to allow hiking and nature study. An interpretive program, including informational plaques situated a key points, could be included in the development or open space plans.

Aesthetic and Scenic Quality Policies

Policy C-6.1

The County shall protect visual access to the bayfront and scenic vistas of water and distinct shorelines through its land use and development review procedures. This viewshed protection is essential for the preservation of Marin County and San Francisco Bay identity, for the enhancement of aesthetic qualities, and for visual and psychological relief from adjacent urban environments.

Comments: The guidelines are in conformance with this policy.

Policy D-1

The criteria for evaluating the design quality of structures in various environmental zones contained in Table 2.3 and the accompanying figures should be used in reviewing proposed actions and in establishing requirements and incentives for developers.

Comments: These design guidelines conform to Policy D-1, specifically Item 6 of table 2.3 for all environmental zones.

Policy D-4

Along creeks, development must retain natural appearance, prevent water pollution, and minimize flood hazards from runoff.

RELATED COUNTY POLICIES AND REGULATIONS

Comments: The guidelines on page I-25, mentioned above, conforms to this policy regarding retaining the natural appearance of the Coyote Creek area.

Policy D-5 On low-lying mudflats or tidal fill areas, public access to creeks, streams, and the shoreline and protection of plant and animal habitats are essential.

Comments: The guidelines on page I-25, mentioned above, conforms to this policy.

Policy D-6 The design of high-intensity nodes of development in the City-Centered Corridor is especially important, because these areas are viewed and visited by large numbers of people. They include countrywide activity centers, small community activity centers, and business development areas such as office concentrations and industrial parks. Business, commercial, and high-density residential development should be concentrated in these centers, shown in the Community Development section of the Plan.

Four major principles should guide the design of countywide and community activity centers and business development areas:

Accessibility. Centers attracting large numbers of people must be well-served by transportation lines, especially by public transit. Walkways should connect buildings conveniently with nearby transit stops. Bicycle and walking paths should connect with adjacent areas, to encourage local access by means other than the automobile.

Concentration. Intensive development must be concentrated at relatively few highly accessible locations. Development along 101 or other major roadways must not take the form of a solid, or even intermittently broken, wall of highly-density development. Rather, there must be widely spaced, compact, fairly dense centers at carefully selected locations, separated by low-density development and open space. This configuration along Marin's major transportation corridor would support efficient transit, stimulate the creation of accessible jobs, and make it possible to reduce sprawl, in addition to providing a pleasant view from the freeway.

Multiple Uses. Uses that are mutually supported will be encouraged. For example, providing housing in business and commercial areas can have the beneficial effects of making services and jobs readily available to residents, supporting a wide range of commercial activities in the evening and on weekends, and increasing the use of public transit. Activity centers should also provide spaces for formal and informal public gatherings.

Amenity. Centers of business and employment must offer an attractive environment, with landscaping, distinctive lighting and outdoor furnishings, and public sitting areas. Public access to water front areas should be provided, and historic buildings and sites should be preserved. The visual impact of parking must be minimized by placing it underground where possible and landscaping surface lots. Views of important natural and man-made features will be preserved and enhanced.

Comments: This plan endorses this policy by providing guidelines for the Junction area.

COMMUNITY DEVELOPMENT

Density and Location of Future Development

Policy B-1 First priority should be given to in filling developed areas and second priority to development on the fringes of developed areas. Development in areas where public facilities are not available or where they would require costly expansion should be

RELATED COUNTY POLICIES AND REGULATIONS

discouraged until such time as comprehensive plans for efficient expansion of public facilities can be implemented.

Comments: The Junction Plan is in conformance with this policy. Policy D-6, mentioned above, speaks to the concentration of uses. The overall concept for the Junction is to create a village center in filling the undeveloped or underdeveloped areas, particularly on the west side of Shoreline Highway between the bridge and the signalled intersection.

Policy B-2

The County seeks to create balanced communities which house and employ all income groups and which provide a full range of facilities and services. Residents should have the opportunity to live and work in the same community, and to fulfill shopping, business, recreational, and educational needs within a reasonable distance of their homes.

Comments: While the Junction area is not large enough to provide development opportunities to house and employ "all" income groups or provide a "full" range of facilities and services, the Junction Plan conforms with the intent of this policy by the creation of a village center with more opportunities than currently available.

Policy B-5

Commercial and higher-density residential development should be in areas where there is high transit accessibility and service capacity, rather than allowing sprawl of continuous strip development along freeway corridors.

Comments: The Junction Plan and these design guidelines conform to this policy. This policy reflects the central concept of the village core.

Policy B-6

Housing should be integrated into commercial and industrial areas where appropriate.

Comments: The Junction Plan recommends that residential uses be incorporated into the village core as one of the potential uses considered as part of proposed new development. Residential units located over commercial uses is an old concept, one that work extremely well in urban core areas throughout the world.

Policy B-7

Development should be discouraged in those areas which have high natural resource values or which pose significant hazards to life or property. Where development is permitted in such areas, the permitted density should be low, and structures should be sited in order to minimize adverse impacts.

Comments: The Junction Plan and these guidelines conform to this policy. The basic concept for the Junction Plan is to create a concentrated village core while restoring the open space and marsh area on the east side of Shoreline Highway.

Policy B-8

Appropriate density bonuses and other incentives should be granted for including low and moderate income housing unit, provided environmental and traffic concerns are met.

Comments: This policy provides a significant opportunity for the Junction area property owners. The Land Use Concept for the Junction recommends that some or all of the parcels east of Shoreline Highway be reclaimed as open space. One method of compensating the east side property owners might include a mechanism (Transfer of Development Rights - TDR) that would allow the owners or potential developers of the west of Shoreline Highway properties the opportunity to purchase the development rights from the east side parcels. With this technique the uses planned for the village core, commercial and residential could be concentrated and a viable economic unit could be generated in the Junction area. Several specific objectives can be met by employing the TDR technique, the property owners on the east side of Shoreline Highway can be compensated for their investment, a concentrated village core

RELATED COUNTY POLICIES AND REGULATIONS

can be created on the west side of Shoreline and permanent open space can be created on the east side of Shoreline.

HOUSING TYPE AND PRICE LEVEL

Policy C-1 A diversity in the range of housing costs should be promoted, and a diversity and mix of housing types and ownership opportunities should be encouraged.

Comments: The Concept of integrating residential units as part of a village core land use program conforms to this policy.

Policy C-2 The supply of moderate and middle income housing should be increased

Comments: The mixed use concept for a village core conforms to this policy.

Policy C-10 The supply of rental units should be preserved and construction of new units should be encouraged.

Comments: The mixed use concept for a village core conforms to this policy.

Policy C-12 An adequate supply of housing for the handicapped should be provided with appropriate access and design features in locations where services and transportation are readily available. Inclusion of such units in new residential developments should be condition of project approval, where feasible.

Comments: The mixed use concept for a village core conforms to this policy.

Policy C-14 Group living facilities should be located near shops, service centers, transit routes, and health centers, but should not be concentrated in any one area.

Comments: The mixed use concept for a village core conforms to this policy.

ECONOMIC DEVELOPMENT

Policy D-4 A diversity of employment opportunities should be available in Marin County in order to provide jobs for residents of all income levels and in order to cushion the County's overall economy against a slump in any one sector against seasonal fluctuations.

Comments: The mixed use concept for a village core conforms to this policy.

Policy D-5 Business activities that meet the employment needs of Marin residents should be encouraged to locate in Marin County.

Comments: The mixed use concept for a village core in the Junction conforms to this policy.

LOCATION OF FUTURE ECONOMIC DEVELOPMENT

Policy E-1 Employment should be encouraged to locate in areas where there exists high transit accessibility, public services, housing to meet employee needs, and complementary retail and commercial uses.

Comments: The mixed use concept for a village core in the Junction conforms to this policy.

RELATED COUNTY POLICIES AND REGULATIONS

- Policy E-3** Services used on a daily or more-than-weekly basis should be located within short distances of housing and other businesses in order to reduce transportation energy costs.
- Comments: The mixed use concept for a village core in the Junction conforms to this policy.
- Policy E-4** Existing commercial and industrial areas should be preserved and expanded. In particular, traditional downtowns should be enhanced so that they continue to add to a community's identity.
- Comments: The primary concept for a village core in the Junction conforms to this policy.
- Policy E-5** Commercial development should be located, sized, and designed to minimize energy consumption on site and to reduce energy used in travelling to an from other destinations. Commercial centers should be designed to provide a variety of services (including park-and-ride facilities for commuters and travelers to regional shopping facilities) that allow energy-efficient and multiple-purpose trips.
- Comments: The village core concept, with a concentration of services and the opportunity to include residential uses, conforms to this policy to reduce trips.

COMMUNITY FACILITIES

- Policy F-4** In general, new development should pay the cost of new infrastructure it requires and the public services it receives. Exact delineation of what constitutes a "fair share" should be the subject of further detailed study.
- Comments: The implementation strategies for the Junction Plan specify that the methods to be used to finance additional infrastructure, and potentially a community center and/or policy substation, include developer fees.

TRANSPORTATION AND ENERGY CONSERVATION

- Policy A-3** In low density areas of the City-Centered Corridor, which are not near already developed service areas, new developments should be encouraged to include shops for food and convenience goods serving local residents, so that long trips will be minimized.
- Comments: The village core concept conforms to this policy.

TRANSPORTATION MODE EMPHASIS

- Policy C-4** Bicycle and walking should be encouraged as significant modes of transportation and should be encouraged for short daily trips to work, shopping, and other activities. Development of a comprehensive system of bicycle routes and amenities should proceed in accord with the Bicycle Plan of Marin adopted by the Board of Supervisors in 1975.
- Comments: The village core concept for the Junction, specifically the concentration of services and the recommendation to establish an open space element with bicycle and pedestrian path links, conforms to this policy.
- Policy C-6** Auto access to the recreational area of West Marin should not be increased. All roads to West Marin are considered scenic highways for County planning purposes,

RELATED COUNTY POLICIES AND REGULATIONS

but official scenic highway designations shall not be sought from the state route because it could encourage added auto use by visitors to Marin.

Comments: The improvements to Shoreline Highway and the Flamingo Road intersection recommended by the Junction Plan are intended to improve the flow of traffic in the Junction area and will not increase access to West Marin.

MAJOR TRANSPORTATION PROJECTS

Administrative Policies

Policy B-2.1 County zoning of unincorporated lands in urban service areas should permit less intensive development than would be permitted by the city unless otherwise mutually agreed upon or specified in an adopted Community Plan. The densities and uses should allow for future development consistent with city land use and development policies. County zoning should be revised to discourage development requiring urban levels of service outside of the urban service area, except where an area is already fully developed in which case the existing zoning would apply.

Comments: The Tamalpais Area Community Plan and the Junction Plan establish the recommended densities and development policies.

Ground Shaking Policies

Policy B-2.1 The development of structures for human habitation, including residential, commercial and industrial use, shall incorporate engineering measures to mitigate against risk to life safety in the areas identified as subject to ground shaking, at least to the extent provided in Title 19, Marin County Code.

Comments: This Policy is incorporated in the Junction Plan by reference.

Policy B-2.2 Applications for development proposed to be sited on landslide deposits, non-engineered fill, or bay mud shall be accompanied by a geotechnical engineering investigation directed to the problem of ground shaking and ground failure. The engineering geologist and civil engineer shall submit recommendations regarding site development, structural engineering, drainage, etc.

Comments: The authorization to require specific engineering reports for new development is found in Marin County Code Titles 19, 20.20, 22.45, 22.47, 22.77 and 23.08. These titles are included in the Junction Plan and Tam Area Community Plan by reference.

Policy B-2.4 The design of structures to be occupied by a large number of people, such as restaurants and hotels, shall accommodate any constraint dictated by the foundation site conditions, as determined by the engineering geologist and civil engineer conducting the site investigation. Such structures shall be designed to be as safe as similar structures in locations not subject to excessive ground shaking or other geologic hazard.

Comments: The Marin County Code Titles referenced in Policy B-2.2 above apply to this policy as well. The Junction Plan and Tam Area Community Plan include the applicable Marin County Code Titles by reference.

Subsidence and Differential Settlement

Policy C-2.1 Filled land which is underlain by compressible materials (bay mud, marsh, slough) should receive special attention during site planning; soils investigations should include borings and sufficient examination to determine the location of former sloughs and

RELATED COUNTY POLICIES AND REGULATIONS

other factors which would accentuate differential settlement; the investigation should delineate those areas where settlement will be greatest, subsidence will occur, etc. and should recommend the site preparation techniques which could be employed to preclude hazard.

Comments: The Junction Plan and these development guidelines assume that the County Planning Department and the Department of Public Works will require the necessary investigations and assurances for projects proposed in the flood plan area of Tam Valley.

Policy C-2.2 In the areas of greater potential for differential settlement, uses should be planned which would not be damaged by such activity and which would provide minimum inducement to settlement which is detrimental to persons, property and public investigation.

Comments: The Junction Plan recommends new development be low scale (two stories) and that the area closest to the original marsh be reclaimed as open space.

Policy C-2.3 Site preparation shall include, where necessary, several years of settlement monitoring, sufficient for detailed foundation engineering and site planning to be based on the site's particular characteristics.

Policy C-2.4 Surcharge may be a necessary site preparation and other mitigating measures designed to accommodate compression and settlement may be required in high risk areas where surcharge is necessary.

Policy C-2.5 Structural design of foundation and utilities shall recognize the potential for differential settlement and subsidence.

Comments: The Department of Public Works and Building Inspection Department have authority to enforce these policies under Marin County Code Titles 19 and 24. These titles are included in the Junction Plan and these design guidelines by reference.

Policy C-2.7 With respect to old or new projects where structures have not been erected, efforts should be made by public agencies to determine the extent of inadequately engineered fills to determine whether or not future risk to property or life exists. Remedial measures which are indicated should be disclosed publicly, and measures and funding of remedy should be proposed. Such measures may include de-watering of fill, clean-out of drainage facilities, load removal from a slide, surface drainage modifications, and maintenance of drainage facilities.

Comments: The RMPC zoning designation for the Junction area allows the County to require environmental documentation for new development projects..

Fire Hazard Policies

Policy C-3.2 Land development and residential building permit applications should be referred to the County Fire Department or pertinent local fire district for review and recommendation.

Comments: The Junction Plan includes the RMPC zoning designation which shall require design review, including a review and recommendation from the Fire district. Marin County Code Titles 16, 19, and 22.47 apply here.

Policy C-3.5 The Board of Supervisors and the appropriate County agencies and all other agencies having fire protection responsibilities should continue to implement the latest Uniform Fire Code.

RELATED COUNTY POLICIES AND REGULATIONS

Comments: Again, the recommendation word "should" tends to confuse this policy. The County does not have a choice regarding implementation of the Uniform Fire Code. The Junction Plan is in conformance with this policy for the same reason.

Flood Hazard Policies

Policy C-4.1 Consider the use of floodplain zoning overlays in flood areas to minimize flooding hazards.

Comments: The guidelines refer to the finished floor elevation for the first floor of new developments to be established by the Land Development Department and the Department of Public Works. This elevation recognizes the flood plain in the Tam Junction area.

Policy C-4.2 Continue to promote multiple uses of areas set aside for flood retention ponding purposes (i.e. agriculture, open space, education, ecology), provide these uses are tolerant for occasional flooding.

Comments: The Junction Plan conforms to this policy with the recommendation to create an area of open space east of Shoreline Highway between the Coyote Creek bridge and the Almonte Boulevard intersection.

Policy C-4.4 Consider adopting an implementable creek setback ordinance to reduce flood damage and protect creek environments in conjunction with the acquisition of drainage easements.

Comments: Policy B-1.3 (Stream Conservation Areas) recommends a 50 foot setback from the top of the bank of the stream in the City-Centered Corridor. The guidelines on page I-25 refers to this policy.

ENERGY

Policy E-2 The energy efficiency of new structures should be increased.

Comments: The guidelines refer to the use of deciduous trees in parking areas for shade in the summer months, while allowing winter heat gain on walls and sidewalks when the trees have lost their leaves.

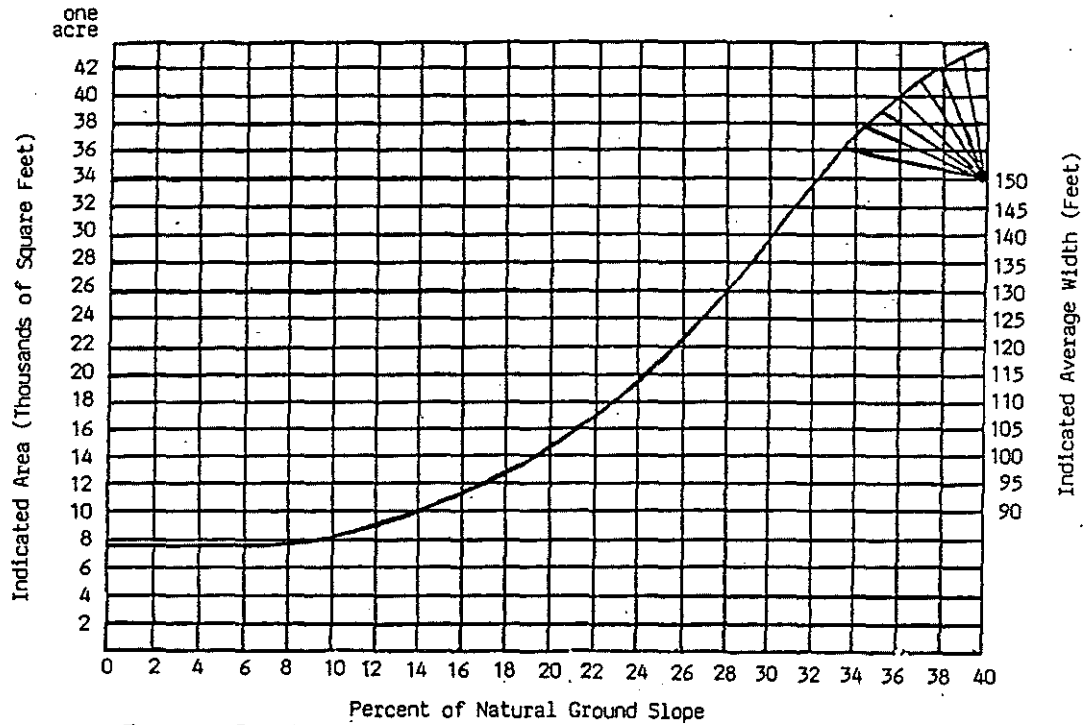
Policy E-3 The number and length of trips should be reduced.

Comments: The Village core concept, which recommends the concentration of commercial uses, including residential, in the Junction area conforms to this Policy.

**APPENDIX J
ORDINANCES**

Slope Chart No. 1 shall apply to all lots in the unincorporated area of Marin County, unless Slope Chart No. 2 or any of the Lot-Slope requirements of Section 22.72.030 apply.

SLOPE CHART NO. 1



Example: For a lot whose ground slope is 28%, the indicated area is 26,000 square feet and the indicated average width is 130 feet.

22.73.030 Special Area Slope Requirements. The following slope requirements are applicable only in the area to which they are specifically applied to the exclusion of any other slope requirements.

1. Sleepy Hollow: This lot slope requirement applies only in the area of the District Zoning Map for Sleepy Hollow, as adopted by the Board of Supervisors by Ordinance No. 784.
 - a. Where the average natural ground slope is 15% or less, the minimum lot area shall be 15,000 square feet.
 - b. Where the average natural ground slope is more than 15%, 1,000 square feet additional lot area shall be added for each additional one percent of slope, to a maximum of 45,000 square feet.
2. Indian Valley: This lot slope requirement applies only in the area of the Indian Valley Specific Plan, as adopted by the Board of Supervisors by Resolution No. 73-119.

<u>Average Natural Slope of Lot</u>	<u>Minimum Lot Size</u>
Less than 10%	1.0 Acres
10% to 20%	1.5 Acres
Over 20%	2.0 Acres

22.73.050 Zoning Regulations. Where the existing zoning requires a lot area or average width greater than that required by this Chapter, then said zoning requirements shall prevail. (Ordinance 2058, 1974)

MARIN COUNTY BOARD OF SUPERVISORS

ORDINANCE NO. 2958

AN ORDINANCE ADOPTING AMENDMENT TO TITLE 22 (ZONING) CODE
SECTION 22.82 (ADDING SECTION 22.82.027) AND AMENDING SECTION 22.82.010

* * * * *

SECTION I. FINDINGS:

- I. WHEREAS the Board of Supervisors held a public hearing to consider amendment to Title 22, adding Section 22.82.027 on October 20, 1987; and
- II. WHEREAS the Board of Supervisors finds the subject amendment to be Categorically Exempt from Environmental Review, per the California Environmental Quality Act, Section 15308; and
- III. WHEREAS the Board of Supervisors finds that these amendments are consistent with State and County Codes; and
- IV. WHEREAS local implementation of these amendments is necessary to protect and enhance public safety; and
- V. WHEREAS these amendments are necessary to establish a more effective process for the development of historic subdivisions, and
- VI. WHEREAS, due to the variability of environmental and physical conditions throughout the County, it is necessary to review the implementation of these amendments within individual areas of the County, and
- VII. WHEREAS to achieve the necessary review of local areas, this ordinance shall become effective only in those areas for which the Board of Supervisors reviews and adopts a Resolution for application of said regulations within a definite area of unincorporated Marin County;

NOW, THEREFORE, BE IT RESOLVED that the Marin County Board of Supervisors does hereby ordain as follows:

SECTION II: Additional language shall be added to Title 22 (Zoning) Code as follows:

Amend Chapter 22.82 by the addition of Section 22.82.027;

Section 22.82.027 Paper Streets, General Purposes and Legislative Findings. Within the unincorporated territory of Marin County, there exists a number of subdivisions which were plotted and recorded prior to the adoption of the County's first subdivision

ordinance on April 3, 1953 (Ordinance 640) and which subdivisions created legal lots of record. However, portions of these subdivisions were not physically developed or improved. In numerous instances the designated streets have not been improved, the streets were mapped without regard to topography, soil conditions, potential or actual slides, presence of drainage ways and other safety concerns; and such streets, where they exist, were not graded and paved and utilities including electric power, water mains, sanitary sewer lines and fire hydrants were never installed. Continuing development and buildout of the subdivisions which contain these paper streets has resulted in less than adequate facilities for fire suppression vehicles, turn-around facilities for fire suppression vehicles, and other service vehicles. There exists inadequate provision for: on-street parking, fire hydrants, drainage facilities, existing vegetation management, soils conditions, landscaping and water courses. The increased buildout rate has resulted in traffic burdens within adjoining and nearby established residential areas due to the absence of a traffic circulation plan when such subdivisions were plotted.

Section 22.82.027(1). Design Review Required for Lots Served by Paper Streets. In those instances where a vacant, unimproved legal lot of record which is accessed by a paper street is proposed for development and improvement, e.g., the construction of a single-family dwelling or any similar site development or improvement, said proposed development or improvement shall be subject to the Design Review requirements set forth in this Section regardless of parcel size or zoning district in which it is located. The scope of the Design Review decision shall include all proposed access improvements.

Section 22.82.027(2) Applicability. The provisions and measures set forth in Section 22.82.027, et seq., are hereby declared to be applicable on a Countywide basis and shall apply in all instances where a lot and/or parcels of record adjoining or abutting a paper street, or lot access, is proposed for development provided, however, that the provisions set forth in Section 22.82.027, et seq., shall not become effective unless and until the Marin County Board of Supervisors adopts a specific Resolution for application of said regulations within all or part of the unincorporated territory of Marin County.

Section 22.82.027(3) Definitions.

a. "Paper street" means, for the purpose of this Section, any street, or portion of a street, road or public vehicular access shown on a subdivision map recorded prior to April 3, 1953, which is undeveloped and/or unimproved, excluding "driveways" as defined in Section 22.82.027(3)(b).

b. "Driveway" means, for the purpose of this Section, any private vehicular access extending from any improved and previously paved access and which length does not exceed two hundred fifty feet (250') or whose longitudinal slope does not exceed twenty percent (20%). In those instances where a driveway is proposed to cross or transverse a paper street right-of-way, the information requirements set forth in Section 22.82.027(5)(a through f) shall govern and apply.

Section 22.82.027(4) Information Required for Design Review Application for Lots Served by Paper Streets. In order to assure compliance with County public health and safety standards when a paper street is proposed for development through the Design Review Application approval process pursuant to the provisions of this Chapter, the following information shall be concurrently submitted, in addition to the standard requirements otherwise set forth in Chapter 22.82 of this Title.

a. Full and complete topographic information relative to the paper street from the point of its proposed connection with an improved street to service the lot (or lots) proposed for development. All ownership, legal interest, or control of all lots and parcels along the street or streets by the applicant shall be disclosed. In those instances where the owner of said lot or lots owns and possesses, controls, or has a legal interest in, additional lots on the same or connecting paper street, full and complete topographic information may be required for the length of the paper street or paper streets which services all the parcels under such ownership, legal interest or control. All such ownership interest or other agreements which potentially access the paper street in question shall be revealed and specified at the time of application.

b. A conceptual grading plan for all access and lot improvements showing existing and proposed contours, cuts and fills, and gradients.

c. The location and amount of all proposed on-street parking facilities and the location of turn-around areas for emergency services vehicles (fire trucks, ambulances and law enforcement vehicles).

d. The location, type and size of fire hydrants and other utility services.

e. A conceptual street design and improvement plan for that portion of the street proposed for development. Additional circulation plans and/or traffic studies may be required to allow evaluation of area circulation, alternative roadway connections, and/or additional street extensions when the length of extension and number or location of connections are determined to possibly result in significant impacts on connecting roads, intersections or the surrounding community.

f. Submission of hydrologic data and hydraulic analysis may be required if the proposed changes in natural grades, drainage, impervious surface, and removal of vegetation could potentially result in drainage impacts on the subject and other properties. If adverse drainage problems are identified, a mitigation plan may also be required.

g. Information required to be submitted in paragraphs a. through f. immediately above shall be reviewed and approved for adequacy by the Director of Public Works.

Section 22.82.027(5) Noticing. Notwithstanding the public noticing requirements set forth in this Chapter, the Planning Director may cause and direct expanded public notice to assure maximum feasible public awareness of any Design Review application processed pursuant to Section 22.82, et. seq.

Amend Section 22.82.010(a) adding the following:

RESIDENTIAL SECOND UNITS IN TAMALPAIS VALLEY AREA

I. GENERAL INFORMATION:

A residential second unit is defined as "one additional living unit on a lot or parcel that is presently developed with an existing primary living unit." For permit processing purposes, there are three (3) kinds of permits for second units.

1. Certificate of Registration: These are required for existing second units that are classified as legal, non-conforming, i.e. second units that were built prior to 1938 or were built when the zoning allowed second units and, the present zoning does not allow second units.
2. Use Permit for Existing Second Unit: These are required for existing second units that were built without a building permit and in violation of the Zoning Ordinance. An application for this type of permit must be submitted by July 10, 1984, or the standards for new second units will be applied.
3. Use Permit for New Second Units: These are required for all new second units, constructed after January 10, 1984 and for all illegal existing second units for which an application is received after July 10, 1984.

II. APPLICATION REQUIREMENTS AND DESIGN STANDARDS:

Second Unit Use Permit/Certificate of Registration application forms may be obtained from the Planning Department. These forms include a complete listing of the required materials and information to be submitted with the application.

Design standards, and setback, height and parking requirements are contained in the Marin County Zoning Ordinance (Title 22), which may be purchased from the Planning Department. Xerox copies of particular sections of the ordinance are available upon request.

III. PROCESS:

Step 1 - Application Submittal

The applicant should submit the completed application with the required fee to the Planning Department. A staff planner will conduct a preliminary review of the application to see if it is complete.

Step 2 - Application Transmitted to Reviewing Agencies

The staff planner assigned to process the application will transmit it to reviewing agencies such as the Department of Public Works, Environmental Health, the Tamalpais Design Review Board and the Tamalpais Community Services District, for review. These groups will have between 2-3 weeks to review the application and either request additional information or make comments. It should be noted that both the Marin Municipal Water District and the Tamalpais Community Services District require separate fees to provide water and sanitary sewer service to the second unit. Please contact these agencies for further information regarding application forms and their current fee schedules.

Step 3 - Public Notice

During the application review process the staff planner will send out Courtesy Notices to owners of property within 300 feet of the project informing them of the application.

Step 4 - Decision

Once an application is complete and all comments have been received a decision will be reached. For Certificates of Registration, the decision will be made by the Planning Director. For all Use Permit applications, a public hearing will be held and the decision will be made by the Zoning Administrator or the Planning Commission.

IV. REQUIRED FINDINGS:

In order for the County to approve any second unit, specific findings or determinations of fact must be made. The required findings for each of the three kinds of second unit permits are as follows:

1. Certificate of Registration:

- A. The structure to be registered shall meet Uniform Housing Code Standards;
- B. The lot or parcel on which the second unit is located has adequate parking;
- C. The second unit has been continuously occupied for a six month period prior to submittal of the application for Certificate of Registration (excepting periods of six (6) months or less).

2. Required Findings - Existing Second Unit:

In order to grant a use permit for a residential second unit existing prior to the effective date of Board of Supervisors Resolution #84-11 (January 10, 1984), the following findings shall be made by the Zoning Administrator, Planning Commission or Board of Supervisors:

- A. The second unit meets all current property development standards of the residential zoning district in which it is located. Existing detached second units shall meet setbacks as established for primary dwelling units and shall not exceed a maximum building height of 15 feet from natural or finished grade. Exception to primary dwelling setbacks and 15 foot maximum height requirements may be considered for existing detached second units by use permit, where findings in accordance with Marin County Code Section 22.88.020(4) can be made.
- B. Primary and secondary residential structures combined shall not exceed a FAR (floor area to lot area ratio) of .3 and the building floor area of the secondary unit shall not exceed 750 square feet. The property owner will be allowed to define which unit is the secondary residential unit. Exception to the maximum square footage of existing second units may be considered by use permit when findings can be made in accordance with Marin County Code Section 22.88.020 (4).

- C. The second unit meets the current Uniform Building Code (UBC) as adopted by the County.
- D. Only one additional unit shall be allowed for each parcel.
- E. The property must meet the minimum lot size specified by the regulating zoning district, including the County Slope Ordinance (Marin County Code 22.73), or the lot area must be greater than 7,500 square feet.
- F. Adequate sanitary services exist for the additional increment of effluent resulting from the second unit as certified by an appropriate agency.
- G. The location of a second unit, including the necessary parking, on a particular property:
 - (1) does not unreasonably infringe on the use and enjoyment of neighboring property with respect to light, air, privacy and view.
 - (2) does not cause excessive noise, traffic congestion, parking congestion or overloading of public facilities.
 - (3) will not result in unsightly development or development that is incompatible with the scale and character of the existing neighborhood.
- H. Adequate parking will be located on the lot for both the primary dwelling and the second unit as follows:
 - (1) A minimum of two (2) off-street parking spaces that conform to Title 24 requirements shall be available for the exclusive use of the residents of the primary unit.
 - (2) A minimum of two (2) off-street parking spaces that conform to Title 24 requirements shall be available for the exclusive use of the residents of the second unit.

The number of required off-street parking spaces for the second unit may be reduced to one (1) if the second unit is less than 450 square feet and is likely to be occupied by only one person.

 - (3) The requirements of (1) and (2) are minimum standards. Additional off-street parking spaces may be required where:
 - a) the size and/or layout of the primary residence and/or second unit make it likely that there may be more than two cars per dwelling, and/or
 - b) on-street parking is very scarce and additional off-street spaces should be provided for guest parking for that lot.

Such required additional parking space may be provided in tandem with the required minimum parking spaces.

- (4) Parking requiring an encroachment permit may be accepted in fulfillment of the required parking as set forth in (1), (2), and (3) above except that encroachment permits for parking shall only be accepted to allow parking on right-of-way immediately adjacent to the boundaries of the subject parcel.
 - (5) The required parking as set out in (1) and (2) may be provided in tandem if it is determined that this will not deter the use of the on-site parking spaces and will not cause parking problems on adjacent roads or result in unsightly or unsafe neighborhood conditions.
- I. The property on which the second unit is located will be owner occupied. Either the primary or secondary unit shall be used by the property owner as the owner's residence. Owner occupancy shall be defined as the maintenance of the home owner's exemption with the County Assessor. The property owner shall file a deed restriction which states that the second unit is permitted only as long as the property is owner occupied. A property owner may request an exemption from the owner occupancy requirement for a period of two years for good cause such as temporary job transfer or settlement of an estate that involves the property. Public notice shall be given prior to a decision of the exemption in accordance with 22.88.020.3. The exemption may be extended for up to two years at a time subject to new public noticing and for each exemption. Exemption may be granted without public hearing.

3. Required Findings - New Second Units

In order to grant a Use Permit for a new Residential Second Unit to be built or created subsequent to the effective date of this resolution, the following findings shall be made by the Zoning Administrator, Planning Commission or Board of Supervisors (22.98.091):

- A. All of the findings required to legalize existing second units.
- B. The second unit has a separate entrance and contains a separate kitchen and bathroom facility.
- C. The addition of a second unit maintains the scale of adjoining residences and blends into existing neighborhood, with respect to building form and height, in particular.
- D. The second unit incorporates low-flow water fixtures and other water saving devices and appropriate energy conservation measures including caulking, weatherstripping, insulation and passive solar design.
- E. The second unit is not proposed for an area subject to hazard from flood or geologic instability.
- F. New detached second units shall meet zoning setback requirements as established for primary dwelling units and shall not exceed a maximum building height of 15 feet from natural or finished grade.

APPENDIX K
LANDS ALONG SHORELINE HIGHWAY DESIRABLE FOR OPEN SPACE

LANDS DESIRABLE ALONG SHORELINE HIGHWAY
FOR OPEN SPACE ACQUISITION

Lands Adjoining Shoreline Highway:

49-012-02

49-012-62

49-012-63

49-012-67

48-202-24 & 26

48-202-12 & 13

48-221-21

48-221-01

48-221-02

48-221-03

48-221-07 & 08

48-221-09

48-232-27 & 28

48-232-29, 30, 36

48-232-32, 33, 37, 38

48-232-34

48-232-39

48-232-40

48-232-42

The following lands are either very large, or adjoin the owners home. Acquisition of appropriate conservation easements would be desirable:

49-011-01 & 02

48-221-10

48-221-06, 20, & 37

48-221-17

48-232-31 & 35

48-232-41